

La Puente

Downtown Business District

Specific Plan

Prepared for:
The City of La Puente
15900 E. Main Street
La Puente, CA 91744

Prepared by:
T&B Planning Consultants, Inc.
3242 Halladay, Suite 100
Santa Ana, CA 92705
(714) 662-2774
Contacts: Frank Greco, Mark Hickner

Adopted June 28, 1994 by Ordinance No. 94-707
Amended June 13, 2000 by Ordinance No. 00-795
Amended February 12, 2002 by Ordinance No. 02-806

TABLE OF CONTENTS

Section		Page No.
1.0	SUMMARY	1.1
1.1	INTRODUCTION	1.1
1.1.1	Purpose and Intent	1.1
1.1.2	General Plan Consistency	1.2
1.1.3	California Environmental Quality Act Compliance	1.2
1.1.4	Relationship to Other Regulations	1.2
1.2	SETTING	1.3
1.2.1	Project Location	1.3
1.2.2	Historical Background	1.3
	FIGURE 1 – REGIONAL MAP	1.4
	FIGURE 2 – VICINITY MAP	1.5
	FIGURE 3 – AERIAL PHOTO	1.6
1.2.3	Existing Conditions	1.7
	FIGURE 4 – EXISTING CONDITIONS	1.8
	FIGURE 5 – EXISTING PARCELIZATION	1.9
1.2.4	Trends	1.10
1.2.5	Opportunities and Constraints	1.11
2.0	SPECIFIC PLAN GOALS, OBJECTIVES AND POLICIES	2.1
2.1	LAND USE	2.1
2.1.1	General Objectives	2.1
2.1.2	Mixed-Use Land Use Policies	2.2
2.1.3	Residential Land Use Policies	2.3
2.1.4	Community Service Land Use Policies	2.3
2.2	CIRCULATION	2.4
2.2.1	General Policies	2.4
2.2.2	Circulation Policies	2.4
2.3	URBAN CHARACTER	2.5
2.3.1	General Characteristics	2.5
2.3.2	Urban Character Policies	2.5
2.4	ARCHITECTURAL DESIGN	2.6
2.4.1	General Objectives	2.6
2.4.2	Architectural Design Policies	2.6
2.5	LANDSCAPE DESIGN	2.7
2.5.1	General Objectives	2.7
2.5.2	Landscape Design Policies	2.7
2.6	URBAN OPEN SPACE	2.7
2.6.1	General Objective	2.7
2.6.2	Urban Open Space Policies	2.7

Section	Page No.
3.0 SPECIFIC LAND USE PLAN	3.1
3.1 GENERAL PROJECT DESCRIPTION	3.1
3.2 SUB AREA PLAN	3.1
FIGURE 6 – LAND USE PLAN	3.2
FIGURE 7 – ILLUSTRATED SITE PLAN	3.3
TABLE 1 – LAND USE STATISTICAL SUMMARY	3.4
3.2.1 Sub Area One	3.5
3.2.2 Sub Area Two	3.5
3.2.3 Sub Area Three	3.6
3.2.4 Sub Area Four	3.6
3.2.5 Sub Area Five	3.7
3.2.6 Sub Area Six	3.7
3.2.7 Sub Area Seven	3.8
3.2.8 Sub Area Eight	3.8
3.2.9 Sub Area Nine	3.9
3.2.10 Sub Area Ten	3.9
3.2.11 Sub Area Eleven	3.10
3.2.12 Sub Area Twelve	3.10
3.2.13 Sub Area Thirteen	3.11
3.2.14 Sub Area Fourteen	3.11
3.3 CIRCULATION PLAN	3.11
FIGURE 8 – EXISTING CIRCULATION	3.12
FIGURE 9 – CIRCULATION PLAN	3.13
3.3.1 Central Avenue	3.14
3.3.2 Main Street	3.14
3.3.3 First Street	3.14
3.4 PUBLIC FACILITIES AND INFRASTRUCTURE PLAN	3.14
3.4.1 Water Service	3.14
FIGURE 10 – PROPOSED MAIN STREET CROSS SECTION	3.15
3.4.2 Sewer Service	3.17
3.5 SCHEMATIC PLAN	3.17
3.5.1 Districts	3.17
FIGURE 11 – SCHEMATIC PLAN	3.18
3.5.2 Nodes	3.19
3.5.3 Enhancement Intersections	3.19
3.5.4 Paths	3.19
3.5.5 Gateway Feature	3.19
3.5.6 View Corridors	3.20
4.0 DEVELOPMENT REGULATIONS	4.1
4.1 GENERAL PROVISIONS	4.1
4.1.1 Purpose and Intent	4.1
4.1.2 Relationship to the La Puente Zoning Ordinance	4.1
4.1.3 Interpretation	4.1
4.1.4 Enforcement	4.1
4.1.5 Amendments	4.2
4.1.6 Variances	4.2

Section	Page No.
4.1.7 Definitions	4.2
4.1.8 Districts	4.2
FIGURE 12 – ZONING MAP	4.3
4.2 MIXED-USE DISTRICT LAND USE REGULATIONS	4.4
4.2.1 Primary Function/Location	4.4
4.2.2 Permitted and Conditional Uses	4.4
4.2.3 Additional Permitted Uses	4.7
4.2.4 Permitted Accessory Uses	4.7
4.2.5 Site Development Standards	4.7
4.2.6 Outdoor Dining	4.8
4.3 OFFICE DISTRICT LAND USE REGULATIONS	4.12
4.3.1 Primary Function/Location	4.12
4.3.2 Permitted and Conditional Uses	4.12
4.3.3 Additional Permitted Uses	4.13
4.3.4 Permitted Accessory Uses	4.13
4.3.5 Site Development Standards	4.13
4.4 BUSINESS PARK LAND USE REGULATIONS	4.15
4.4.1 Primary Function/Location	4.15
4.4.2 Permitted and Conditional Uses	4.15
4.4.3 Additional Permitted Uses	4.16
4.4.4 Permitted Accessory Uses	4.16
4.4.5 Site Development Standards	4.16
4.5 COMMUNITY SERVICE DISTRICT LAND USE REGULATIONS	4.18
4.5.1 Primary Function/Location	4.18
4.5.2 Permitted and Conditional Uses	4.18
4.5.3 Additional Permitted Uses	4.19
4.5.4 Permitted Accessory Uses	4.19
4.5.5 Site Development Standards	4.19
4.6 MULTI-FAMILY RESIDENTIAL DISTRICT LAND USE REGULATIONS	4.21
4.6.1 Primary Function/Location	4.21
4.6.2 Permitted and Conditional Uses	4.21
4.6.3 Additional Permitted Uses	4.21
4.6.4 Permitted Accessory Uses	4.21
4.6.5 Site Development Standards	4.22
4.6.6 Senior Citizen Housing Developments	4.23
4.7 PARKING STANDARDS	4.24
4.7.1 Purpose	4.24
4.7.2 Basic Regulations for Off-Street Parking	4.24
4.7.3 Parking Area Design Standards	4.24
TABLE 2 – OVERALL PARKING BAY WIDTH	4.26
4.7.4 Parking Requirements	4.28
4.7.5 Special Requirements	4.30
4.7.6 Modified Parking Requirements	4.30
4.7.7 Parking In Lieu Fees	4.31
4.7.8 Public Off-Street Parking Districts	4.28
4.8 DISPLAYS ON PUBLIC SIDEWALKS	4.32
4.8.1 Purpose and Intent	4.32
4.8.2 Location and Design Standards	4.32
4.8.3 Permit Required	4.33

Section	Page No.
5.0 DESIGN GUIDELINES	5.1
5.1 GENERAL SITE PLANNING GUIDELINES	5.1
5.1.1 Mixed-Use Development	5.1
5.1.2 Office and Light Industrial Development	5.2
5.1.3 Multi-Family Residential Development	5.3
5.1.4 Defensible Space	5.3
5.2 ARCHITECTURAL GUIDELINES	5.3
5.2.1 Residential and Non-Residential Structures	5.4
5.2.2 Additional Non-Residential Design Guidelines	5.6
5.2.3 Additional Residential Design Guidelines	5.7
5.2.4 Examples of Architectural Design Elements	5.8
5.3 PARKING GUIDELINES	5.8
5.3.1 Parking Criteria	5.9
5.3.2 Off-Street Parking and Loading Design	5.10
5.4 LANDSCAPE DESIGN GUIDELINES	5.13
5.4.1 Streetscape Design Concepts	5.13
5.4.2 General Landscape Design Guidelines	5.14
5.4.3 Recommended Street Tree Plant Palette	5.16
5.5 BUILDING SIGNAGE	5.17
5.5.1 Signage Criteria: Non-Residential	5.17
5.5.2 Signage Criteria: Residential	5.19
5.5.3 Promotional Advertising Devices	5.20
5.5.4 General Sign Regulations	5.20
5.6 LIGHTING GUIDELINES	5.20
5.7 DEVELOPMENT/DESIGN REVIEW	5.20
5.7.1 Project Requiring Development/Design Review	5.21
5.7.2 Authority	5.21
5.7.3 Application for Development/Design Review	5.23
5.7.4 Development/Design Review Procedure	5.23
5.7.5 Findings	5.25
5.7.6 New Applications Following Denial	5.25
6.0 PLAN IMPLEMENTATION	6.1
6.1 SPECIAL EVENTS	6.1
6.2 SPECIAL INCENTIVES	6.1
6.3 SMALL LOT CONSOLIDATION	6.2
6.4 ECONOMIC DEVELOPMENT STRATEGIES	6.3
6.4.1 Creation of the DBD Redevelopment Project Area	6.3
6.4.2 Attraction of Desired Uses	6.3
6.4.3 Small Business Assistance Program	6.4
6.4.4 Implementing and Financing Alternatives	6.4
6.5 PROJECT PHASING	6.6
TABLE 3 - PHASING PROGRAM	6.6
FIGURE 13 - PROJECT PHASING PLAN	6.7
7.0 GENERAL PLAN CONSISTENCY	7.1

APPENDIX – Examples of Encouraged Architectural Design Elements

1.0 SUMMARY

1.1 INTRODUCTION

1.1.1 Purpose and Intent

This Specific Plan has been prepared for the purpose of establishing guidelines for the intensification and redevelopment of the Downtown Business District (hereinafter the "DBD") in the City of La Puente, California. This document provides the City with comprehensive plans, policies and regulations to ensure efficient, orderly redevelopment in accordance with the adopted La Puente General Plan. Although the La Puente Downtown Business District Specific Plan (hereinafter the "Specific Plan") is very specific relative to uses, siting, densities and land use mix, this Specific Plan builds in a certain level of flexibility to accommodate innovative applicant-developer approaches and changing economic and market conditions.

Presently, the DBD is in a state of decline. The appeal of the area to retailers and consumers has weakened due to the lack of a major arterial highway or other attracting element in the downtown. The Specific Plan has been prepared to guide the City of La Puente in rejuvenating the vitality of the DBD by:

- Creating a foundation for a revitalized retail base;
- Encouraging the creation of a job center;
- Establishing diverse civic and community services;
- Enhancing the visual appeal of the DBD; and
- Providing residential opportunities.

The State of California (under California Government Code, Title 7, Division 3, Articles 8 and 9, Sections 65450 and 65507) encourages cities to adopt Specific Plans either by resolution to establish a policy document or by ordinance to establish a regulatory document. This Specific Plan document is intended to be a regulatory document and is therefore subject to Planning Commission review and City Council adoption by ordinance.

All future development plans, site plans, tentative tract and parcel maps and other similar entitlements for properties located within the boundaries of this Specific Plan shall be consistent with the regulations set forth in this document and with all other applicable City regulations. Furthermore, all regulations, conditions and programs contained herein shall be deemed distinct and independent provisions of the Specific Plan. If any section, clause, phrase or portion thereof is for any reason held to be invalid by the decision of any federal or state court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Specific Plan.

1.1.2 General Plan Consistency

The Specific Plan has been prepared in accordance with City of La Puente's General Plan and related policies, regulations and programs. The various land uses permitted by this document are consistent with the goals, objectives, policies and general land uses described in the General Plan. This Specific Plan focuses directly on the DBD providing "tailored" design guidelines and development regulations specifically intended to address issues related to the downtown. Section 7.0 of this document addresses specific compliance to the General Plan.

1.1.3 California Environmental Quality Act Compliance

In compliance with the requirements of the California Environmental Quality Act (CEQA) and City of La Puente environmental procedures, an Environmental Impact Report (EIR) has been prepared for the Specific Plan area in conjunction with the preparation of the Draft Specific Plan. The accompanying EIR addresses the potential environmental impacts which may result from the implementation of the Specific Plan and proposes measures to mitigate potential impacts.

The EIR will be utilized in the processing of individual projects within the Specific Plan area as a "Program EIR" as provided for in Section 15168 of the CEQA. It is anticipated that individual projects will be subject to environmental review by the City of La Puente to determine consistency with the project as described in this document. Where individual projects are consistent with this document, further environmental review will not be necessary and the mitigation measures included in the accompanying EIR may be applied. However, where individual projects may result in impacts beyond those examined in this document, further environmental review (possibly in the form of an expanded Initial Study or EIR) may be required and additional mitigation measures may be imposed.

1.1.4 Relationship to Other Regulations

This Specific Plan will provide the user with most of the information needed to determine which City policies, standards and regulations will guide the development of a particular parcel. Areas not specifically covered by the Specific Plan will continue to be governed by existing City regulations. No provision in this document is intended to repeal, abrogate, annul, impair or interfere with any existing City ordinance, except as specifically repealed by adoption of the Specific Plan.

Whenever the provisions of this Specific Plan impose or establish more restrictive regulations than those imposed by any City ordinance, code or regulation, the provisions of this document shall govern. The stipulations of this document are not intended to interfere with or abrogate any easements, covenants or other existing agreements which are more restrictive than this document.

1.2 SETTING

1.2.1 Project Location

The Specific Plan area comprises the DBD within the City of La Puente. The City of La Puente is located in the East San Gabriel Valley in Los Angeles County, California, approximately 22 miles northeast of downtown Los Angeles (see Figure 1, *Regional Map*). The City is bordered along the north by West Covina, on the east and south by the City of Industry and along the west by unincorporated County land. Access to the DBD is available from the Pomona Freeway (State Route 60) to the south by way of Hacienda Boulevard. In addition, access to the site is available via the San Bernardino Freeway (Interstate 10).

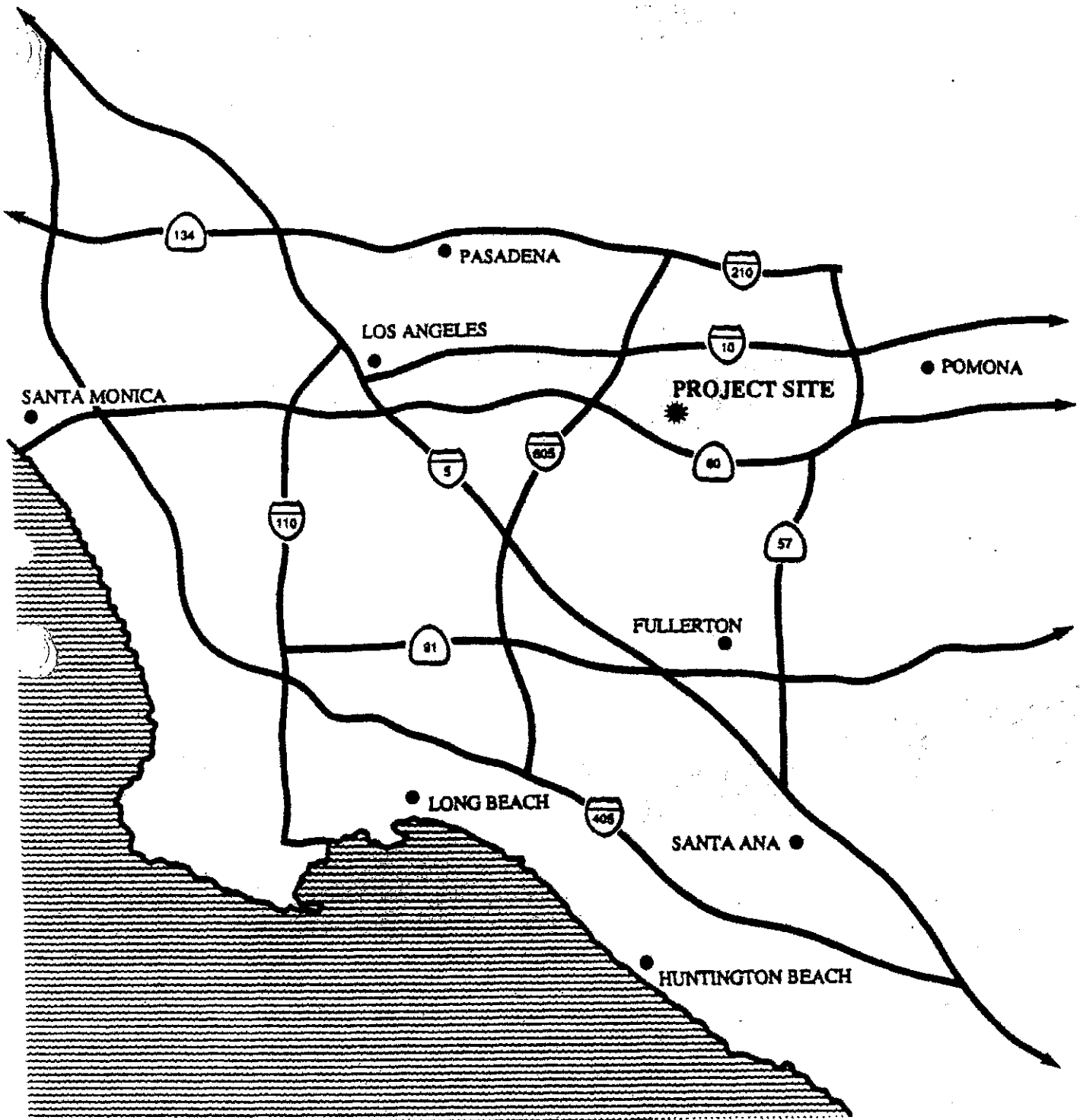
The Specific Plan area is defined by Glendora Avenue on the west and Fifth Street on the east, Old Valley Boulevard and the Southern Pacific Railroad right-of-way on the south, and Rowland Street on the north (see Figure 2, *Vicinity Map*, and Figure 3, *Aerial Photo*). The railroad right-of-way also serves as the City of La Puente's southern boundary.

1.2.2 Historical Background

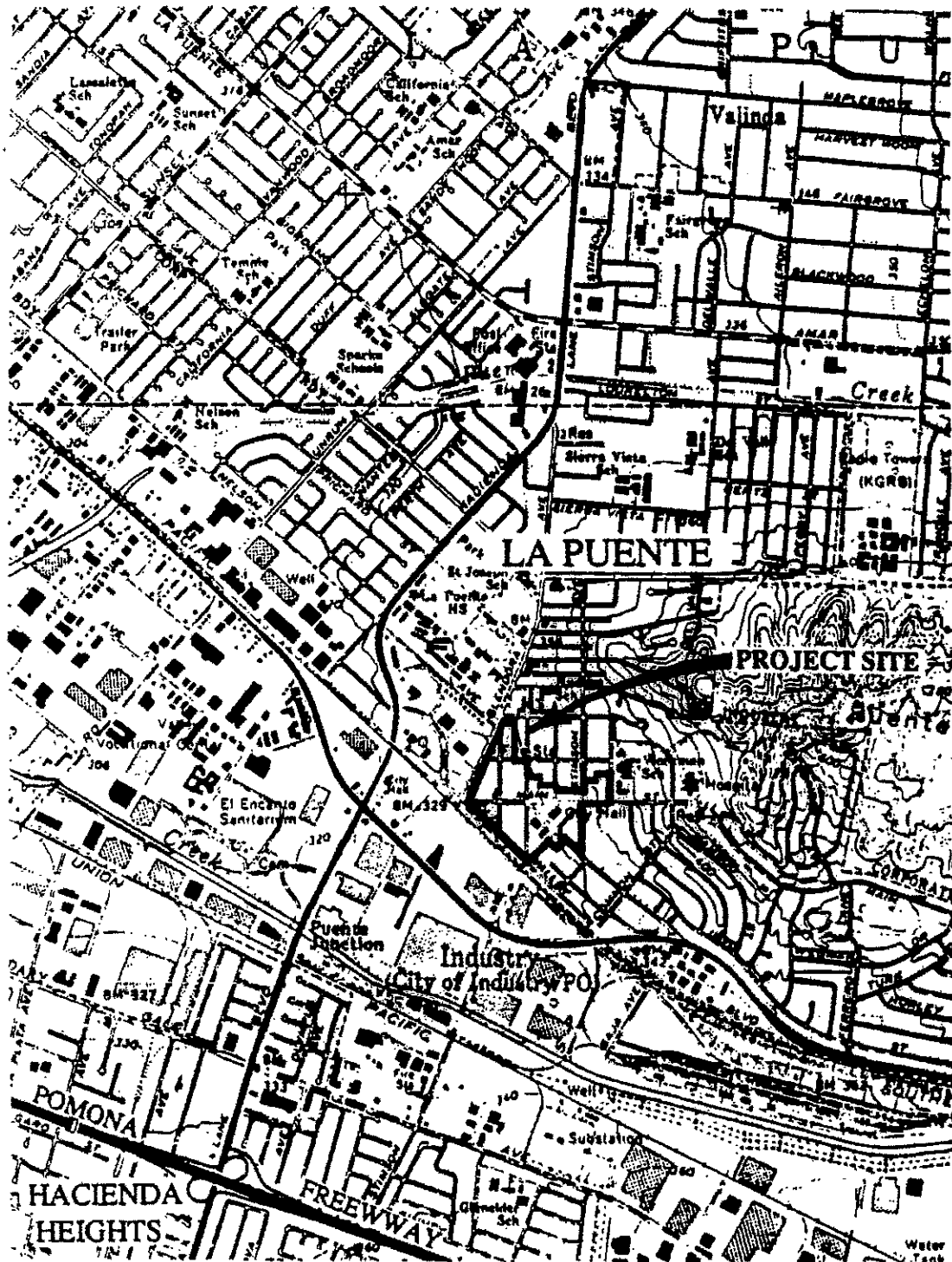
La Puente's DBD is part of the original Town of La Puente subdivision originally recorded in 1886. La Puente is still platted as a part of the Rancho La Puente, formally a part of the Mission San Gabriel established in the 1770's. Rancho La Puente was characterized by gently rolling grasslands punctuated by large oak and alder trees. San Jose Creek, which traversed through the Ranch, was distinguished by green marshes and banks covered with willows, wild grapes, blackberry vines and wild roses. Agricultural uses began to replace ranching with the construction of a railroad through the area in 1875. By 1890, the Rancho La Puente had developed into an almost self-supporting, independent community. Grape vineyards and wheat were both very valuable crops in the area and cotton and barley were also raised.

The City of La Puente was incorporated in 1956. A major portion of development within the city limits occurred during the 1950's and 1960's. In fact, over 90 percent of the City's dwelling units were constructed between 1950 and 1964 to accommodate the post-war baby boom.

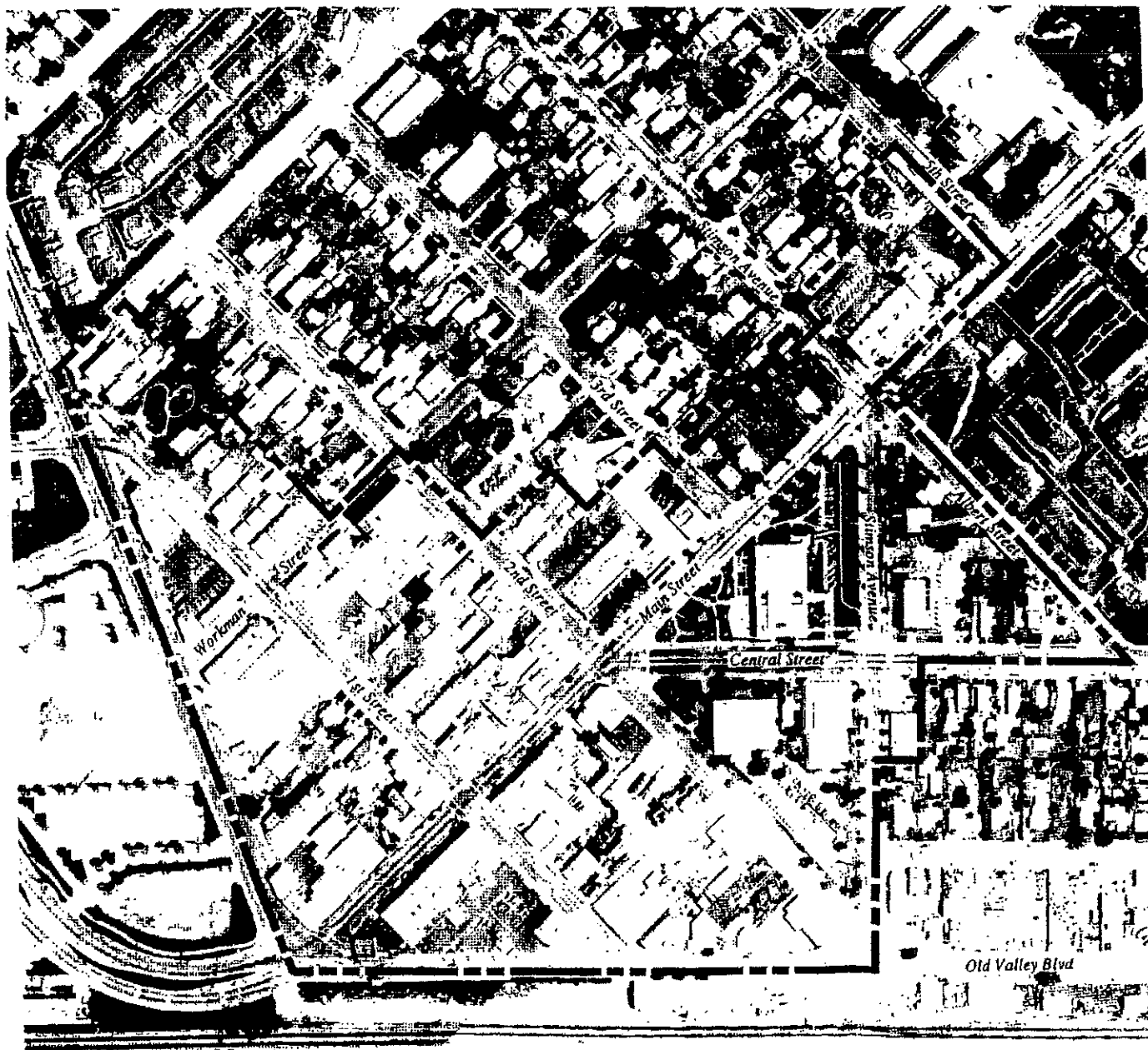
The present-day City of La Puente's downtown area was a part of the original Town of La Puente subdivision and, as such, contains many of the oldest buildings in the City. The DBD's extensive history in the early development of Southern California evokes a small town atmosphere. The DBD has a long history of being used for retail commercial uses, especially along Main Street, and much of the commercial development within the DBD occurred during the first part of the Twentieth Century. As new commercial businesses were developed along Hacienda Boulevard



REGIONAL MAP
FIGURE 1



VICINITY MAP
FIGURE 2



AERIAL PHOTO
FIGURE 3



and Amar Road, the DBD has experienced a decline. Recently, the on-grade crossing of Stimson Avenue over the Southern Pacific Railroad tracks was closed and traffic was rerouted to Hacienda Boulevard.

1.2.3 Existing Conditions

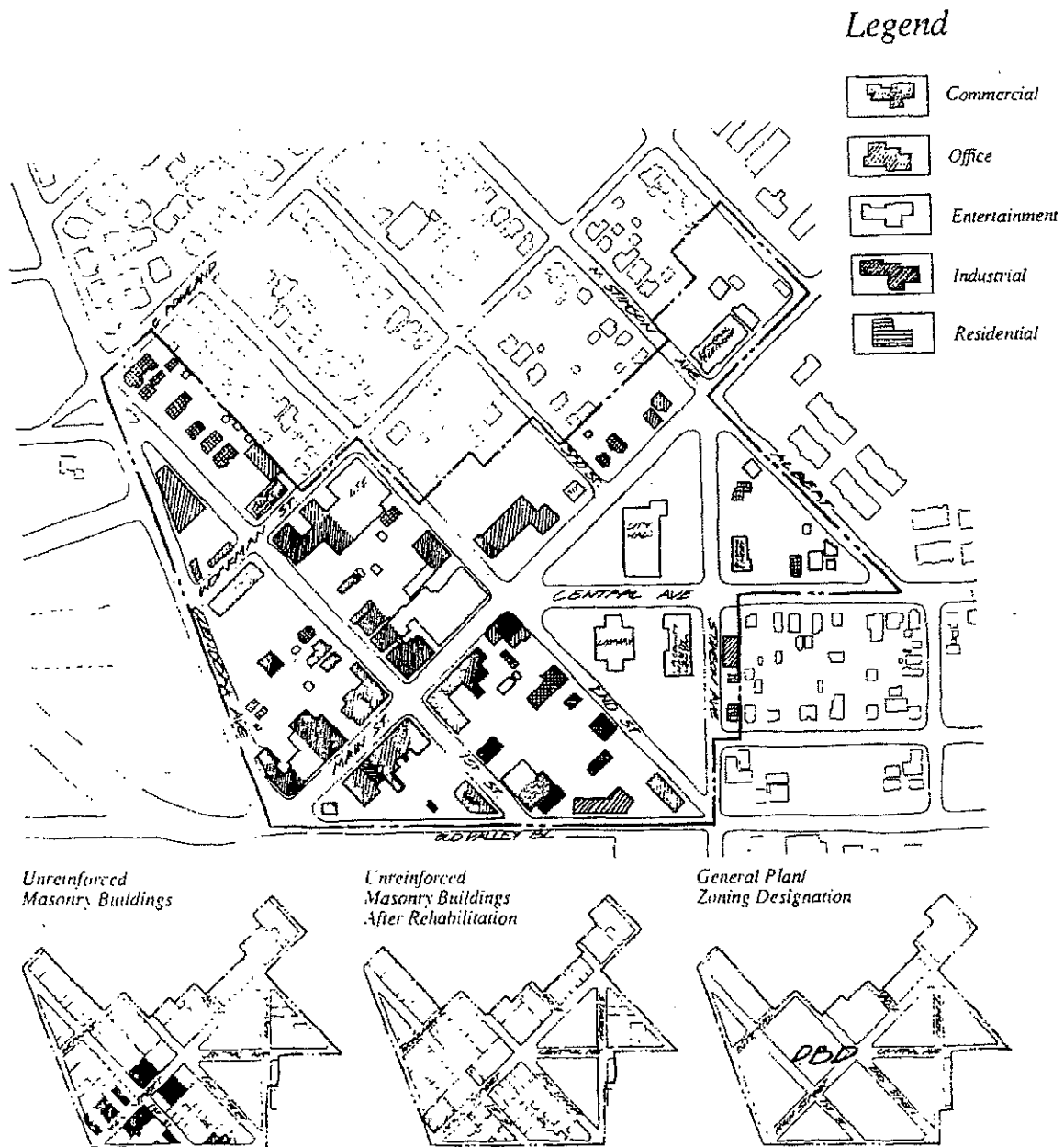
La Puente is an inland residential city which is 96 percent developed. The City is devoid of significant biotic, zoologic, or geologic natural features. At present, the City has a total planning area of approximately 2,230 acres (3.48 square miles). The Specific Plan area encompasses approximately 23.7 net acres, less than one percent of the City's total land area. Figure 4, *Existing Conditions*, depicts the existing land uses within the project area.

The City is a "bedroom" community, with residential uses comprising more than 70 percent of the City's land area. Of that total, single-family units total 91 percent of the housing inventory. The remaining nine percent of residential land is developed with multi-family units which comprise 38 percent of the City's total number of housing units. A 60-unit mobile home park also is located within the city limits.

There are six neighborhood commercial centers in La Puente. Gross floor area within these centers ranges from 105,750 to 142,700 square feet. Commercial strip developments are located along major highways and arterial streets such as Amar Road, Hacienda Boulevard and Azusa Avenue. This type of development underutilizes lots, leaving shops and businesses that do not face directly onto the highway or street vacant of tenants. The light industrial and manufacturing base of the City is relatively small when compared to neighboring communities, especially the City of Industry. The industrial and manufacturing sector is not a significant land use within the City of La Puente.

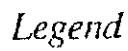
At present, a variety of uses, including approximately 26 single-family residences, occupy the Specific Plan area. The majority of establishments range from general merchandising to office and service related businesses and include such uses as antique stores, bakeries, thrift outlets, barber shops, apparel stores and small offices. Most of the land area is divided into small, individual ownerships (see Figure 5, *Existing Parcelization*), and the DBD does not contain any large department or variety store or large anchor tenant. Many vacant storefronts exist within the DBD, a testament to the general decline of commerce within the area.

Within the limits of the Specific Plan boundary, there are two other Specific Plan areas. Both of these plans (Specific Plan Numbers 82-1 and 84-1B) have been approved by City Council. SP No. 82-1 is a triangular parcel located at the western entrance into the DBD on the south side of Main Street. This Specific Plan area is bounded by East Main Street on the north, South First Street on the east, and Old Valley Boulevard on



EXISTING CONDITIONS
FIGURE 4





City-owned property

Los Angeles County Owned
Property

the southwest. As part of SP No. 82-1, a public parking lot has been constructed.

SP No. 84- 1B contains approximately 3.8 acres. East Main Street, South Second Street, Old Valley Boulevard and South First Street form the project boundaries. To date, two buildings have been erected. These buildings house automobile service and warehousing uses. In addition, public off-street surface parking has been made available within this Specific Plan. Both SP No. 82-1 and SP No. 84- 1B received Negative Declaration statements from the City; Environmental Impact Reports were not prepared for either project. The La Puente Downtown Business District Specific Plan will supersede both SP No. 82-1 and SP No. 84-1B.

1.2.4 Trends

The DBD is in a state of decline with several existing buildings, most significantly along Main Street, burdened by vacancies. Because no arterial highways pass through the DBD, the visibility of commercial and office uses in Downtown La Puente is severely limited. According to the La Puente Land Use Element, the *"Downtown area is not realizing its economic potential. Deteriorated buildings, inefficient circulation and lack of visual amenities detract from the commercial appeal of the area."* Current trends include:

Trends

- Lack of identity as the historical heart of La Puente.
- Decline in downtown commerce.
- Decline of pedestrian-oriented land uses.
- Degradation in the visual appearance of the DBD.
- Piecemeal development resulting from multiple land divisions and ownerships.
- Declining sales tax revenues due to rising vacancy rates.
- Decline in commercial rents.
- Lack of projected residential growth within the DBD.
- Lack of diverse and community-oriented shopping opportunities.
- Lack of design consistency.

If these trends are not dealt with effectively, the vitality of the downtown will continue to decline and current downward conditions in the area will be exacerbated. The City wishes to reverse this trend by encouraging intensification and redevelopment and improving physical and economic conditions within the DBD. This Specific Plan has been designed to preserve the positive attributes of the DBD and create opportunities to alter the above trends in a way which will benefit the City of La Puente as a whole.

1.2.5 Opportunities and Constraints

A primary task in any planning assignment is to conduct an opportunities and constraints analysis, which considers the physical and environmental attributes and limitations that the site and surrounding area offers. The opportunities and constraints analysis thus forms the basis on which the ultimate planning concepts are developed. Extensive thought was given to how best develop the DBD in a manner that would build upon the qualities and character of the community. The primary objective of the Specific Plan is to blend new development into the community fabric while simultaneously protecting and preserving the small-town character and charm of the DBD.

Opportunities

The La Puente DBD contains the following opportunities:

- The existing sidewalk system, parallel parking stalls and ample street width allow opportunities to expand sidewalks and "pinch" intersections to promote pedestrian safety.
- Areas containing larger parcels and City-owned parcels are conducive to redevelopment efforts.
- Many City-owned parking lots can provide convenient off-street parking opportunities.
- Land area along Main Street is indicative of traditional American main streets, and buildings can be designed to further this notable historic theme.
- Significant historical structures exist Within the DBD, which can be preserved to create a traditional, small town atmosphere.
- Street corners provide opportunities to create visual amenities.

- The existing civic center bounded by Second Street, Main Street and Stimson Avenue can be established as a focal point for the community.

Constraints

To take advantage of the above opportunities, an extensive effort was undertaken to incorporate tools within the Specific Plan that will overcome or alleviate existing development constraints.

These constraints include the following:

- The DBD lacks significant landscape features designed to create a distinct image.
- Existing street trees and other streetscape amenities are minimal.
- Signage within the DBD lacks consistency in terms of size, illumination, materials and purpose.
- Small, individual land ownerships are not conducive to lot consolidation.
- Existing buildings lack character due to inappropriate facade treatment.
- "Creeping commercialism", which promotes strip commercial configurations, exists contiguous to Main Street, First Street and Stimson Avenue.
- Central Avenues termination at Second Street creates an inefficient intersection.
- Condemnation will not be available as a tool to assist in implementing the Specific Plan.

2.0 SPECIFIC PLAN GOALS, OBJECTIVES AND POLICIES

Because of the unique qualities and characteristics of the La Puente DBD, it is apparent that the existing General Plan goals cannot be fully realized in the downtown through the use of existing ordinances. Potential concerns in the areas of land use, circulation, urban design and infrastructure have been identified by the City and will be addressed in this Specific Plan. The overall goal of the La Puente DBD Specific Plan is to:

Provide for the implementation of the General Plan, with emphasis on special topics unique to the planning area including: Land Use, Circulation, Urban Design and Infrastructure. Encourage, through public and private investment, the development of a downtown business district that is a cohesive, functional and aesthetic whole.

2.1 LAND USE

For purposes of this section, land uses, proposed in this Specific Plan will be categorized into Mixed-Use, Office, Business Park, Residential and Community Service land use categories or "districts". The intent of the La Puente Downtown Business District Specific Plan is to :

2.1.1 General Objectives

- Provide a viable mix of commercial, office, community service and multi-family residential uses within the DBD, consistent with the City of La Puente General Plan.
- Encourage private sector investment in the DBD.
- Organize and develop a well-designed and functional downtown which meets the needs of a diverse population and provide new development that is compatible with existing land uses.
- Encourage rehabilitation, renovation and intensification of existing commercial structures.
- Promote infill development that is compatible with existing land uses and structures.
- Encourage cooperation between the public and private sectors in the revitalization of the DBD.
- Promote the establishment of a downtown and community center that reflects the heritage of La Puente and preserves the small-town charm of the DBD.

2.1.2 Mixed-Use Land Use Policies

- Promote high quality, attractive and functional site planning and architecture for commercial, office, light-industrial and multi-family residential development with the DBD.
- Encourage the first floor of buildings on Main Street between Glendora Avenue and Third Street be used for specialty retail businesses that serve area residents and employees. Ground floor uses should be reserved for specialty retail and entertainment uses which depend on pedestrian traffic.
- Encourage community servicing uses such as theaters, youth clubs and facilities, a senior center, a police station and day care facility to locate within the DBD.
- Locate offices on the fringes of the downtown or above the first floor of buildings on Main Street.
- Group and cluster commercial development in forms and patterns which promote an interesting and varied streetscape and encourage pedestrian traffic. Promote greater building mass contiguous along Main Street between Glendora Avenue and Third Street.
- Encourage a variety of multi-family housing opportunities within the DBD including townhomes, senior citizen housing, affordable housing, apartments, SROs and stacked flats.
- Investigate opportunities for parcel consolidation and elimination of marginal land uses.
- Create a regional draw through the provision of special events which focus attention on the DBD.
- Promote specialty uses and events unique to the DBD which emphasize the cultural heritage of La Puente.
- Intensify and redevelop underutilized parcels for buildings and open space and promote infill development, where possible.
- Encourage office and commercial architectural styles for all uses within the DBD that complement the heritage of La Puente.
- Encourage the development of specialty commercial facilities which complement the streetscape, are designed at a human scale and are pedestrian-oriented.

- Promote neighborhood and community serving uses and commercial and office developments that do not encourage vehicular traffic through nearby existing residential neighborhoods.
- Create a symbiotic and mutually beneficial relationship between the commercial and residential uses in the DBD.
- Develop a series of comprehensive design and development standards that compliment the heritage and desired image of Downtown La Puente, while accommodating growth and intensification of land uses.

2.1.3 Residential Land Use Policies

- Encourage residential development adjacent to existing residential neighborhoods.
- The focus or design orientation of residential development within the DBD shall reflect an urban image in required building materials, setbacks, architectural mass and density that reflect the greater level of intensification planned for the DBD.
- Locate residential land uses on the fringe of the downtown or above the first floor of buildings on Main Street.
- Establish a comprehensive series of design guidelines and standards for residential development and redevelopment.

2.1.4 Community Service Land Use Policies

- Encourage public and quasi-public land uses adjacent to the Civic Center complex, which is bounded primarily by Central Avenue, Albert Street and Main Street.
- Construct areas for public gathering adjacent to retail commercial uses and the Civic Center complex.
- Utilize public facilities, such as the Civic Center complex, to provide a focal point for the DBD and to establish the social, commercial and cultural hub of the community.
- Maintain the downtown as the primary location of civic activity.
- Promote special events such as farmers markets and street fairs which encourage public participation.

2.2 CIRCULATION

2.2.1 General Objectives

- Improve vehicular and pedestrian access to commercial enterprises located within the DBD.
- Improve on-street and off-street parking facilities.

2.2.2 Circulation Policies

- Investigate the opportunity of establishing an off-street parking district.
- Investigate other forms of parking configurations, such as diagonal parking, which may increase on-street parking capacity.
- Investigate the feasibility of reopening Stimson Avenue to facilitate movement into the DBD.
- Consider allowing shared parking facilities for uses that operate at different times of the day or night.
- Promote pedestrian circulation by providing improvements such as development of a civic center plaza and "pinched" intersections, which accommodate safe pedestrian movements.
- Reserve Main Street curbside parking for downtown shoppers; people who work downtown should be encouraged to use spaces along streets surrounding the heart of the downtown or in public and private parking lots.
- Encourage metered parking along Main Street and other streets within the DBD.
- Promote reciprocal access between individual parcels designed to accommodate on-site parking bays.
- Dedicate land and provide easements designed to accommodate vehicular access alleyways and pedestrian walkways where possible.
- Reclassify streets by specific function and downscale where possible.
- Provide pedestrian amenities such as seating, light fixtures and pavement treatments designed to accommodate pedestrian movements.

- Encourage pedestrian circulation through the use of a paseo system designed to link the DBD to adjacent residential neighborhoods and public parking areas.

2.3 URBAN CHARACTER

2.3.1 General Characteristics

- Design and coordinate development within the DBD to complement the heritage of La Puente, create an attractive physical setting, foster a sense of identity and enhance the image and quality of life in the City.
- Avoid the lack of community identity associated with many communities by instituting a series of design standards and regulations to reinforce the sense of community identity in the DBD.
- Promote a traditional small-town image in the DBD which portrays it as a diverse, yet well maintained and pleasant urban environment in which to live and do business.
- Create an overall design structure for all development within the DBD that will establish a visually perceivable and unique image throughout the entire planning area.
- Increase commercial activity by improving the visual character and functional efficiency of the DBD.

2.3.2 Urban Character Policies

- Reinforce the DBD's identity and character through the use of classic American storefront architecture.
- Create a sense of continuity throughout the DBD through appropriate landscape architectural features.
- Investigate land uses and circulation features which promote contiguous streetscapes oriented to the pedestrian, in human scale.
- Require special gateway landscape and design treatments at the entrance into the DBD at the intersection of Main Street and Glendora Avenue.
- Reinforce the heritage of the community through the recognition of historical urban design patterns, landscape features and images steeped in the traditions of American storefront architecture.

- Promote land use and design controls which foster the traditional small town Main Street theme.
- Create a distinction between the DBD and other neighborhoods through the use of formal and more urban oriented landscape and architectural themes.
- The character of the DBD should vary in response to differences in the intensity of the proposed land uses yet be compatible with existing surrounding neighborhoods.

2.4 ARCHITECTURAL DESIGN

2.4.1 General Objectives

- Architectural designs and concepts should be guided by criteria which reinforce the sense of community identity, avoid the feeling of sameness and blandness, and enhance the traditional character of the La Puente DBD.
- Reduce the economic risk of rehabilitating or developing structures in the DBD.
- Eliminate substandard and deteriorating structures by replacing unsightly, obsolete and unsafe structures with new buildings.
- Conserve, rehabilitate and revitalize existing buildings of historical significance or aesthetic importance throughout the DBD, where possible.

2.4.2 Architectural Design Policies

- Reinforce community identity through the application of consistent design guidelines related to commercial, residential, office, light industrial and public structures, uses and other focal areas.
- Incorporate architectural features into buildings within the DBD to reinforce the history and heritage of the downtown La Puente area.
- Ensure quality development by creating architectural design guidelines that provide clear direction to store owners, builders and architects.
- Promote the upgrading of buildings through rehabilitation and new construction; consider the National Main Street Program.

2.5 LANDSCAPE DESIGN

2.5.1 General Objectives

- Design attractive streetscapes that enhance the visual and aesthetic qualities and contribute to a high quality, memorable experience.
- Landscape designs should promote streetscape continuity and preserve the heritage of La Puente.

2.5.2 Landscape Design Policies

- Develop mandatory standards relative to tree type, size and spacing for streets, parking lots and public gathering spaces.
- Develop design guidelines for landscape buffer areas adjacent to existing residential neighborhoods.

2.6 URBAN OPEN SPACE

2.6.1 General Objective

- Promote the establishment of public urban open space with the La Puente DBD.

2.6.2 Urban Open Space Policies

- Implement measures to retain sufficient amounts of public open space within the DBD.
- Encourage innovative design concepts and landscape techniques to promote useable urban open space.
- Promote open space amenities throughout the DBD to enhance the visual quality of the environment.
- Encourage the establishment of a small park, plaza, or town square designed to accommodate public events.
- Investigate opportunities for accommodating pedestrian movements through the establishment of a plaza and enhancement of sidewalk facilities.

3.0 SPECIFIC LAND USE PLAN

3.1 GENERAL PROJECT DESCRIPTION

The La Puente DBD Specific Plan is designed to retain the existing small town, urban character of the City of La Puente, while promoting intensification and redevelopment to improve physical and economic conditions within the DBD. To achieve a well-balanced, economically viable and attractive downtown, intensification and redevelopment should incorporate both public and private efforts. Not only should buildings be refurbished and upgraded, but improvements to the downtown circulation system and streetscape also must be incorporated into the overall scheme. The City of La Puente's DBD retains much of the charm of small town America in the 1930's, 40's and 50's.

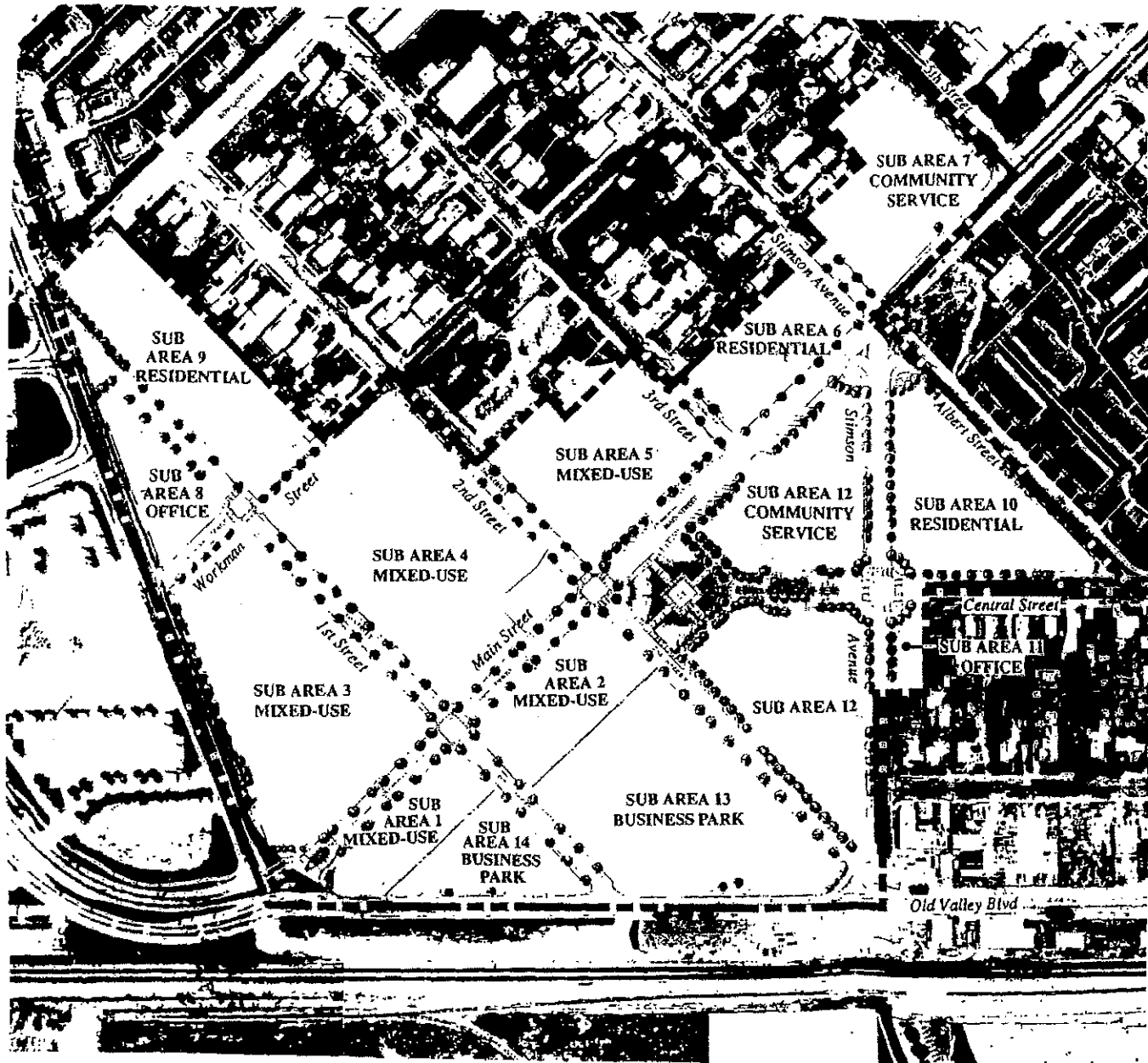
As depicted in Figure 6, *Land Use Plan*, and Figure 7, *Illustrated Site Plan*, the Specific Plan proposes a combination of retail, mixed-use, office, light industrial and residential uses, as well as a variety of community service and civic structures, in the DBD. Commercial activity will be concentrated along Main Street where storefront facades will be strengthened and the streetscape will be enhanced to provide a pleasant, pedestrian-oriented shopping atmosphere. Main Street will be primarily characterized by one, two and possibly three story commercial establishments which frame the street with little or no setbacks. The second and third floors of these structures will be targeted for use as office or multi-family residential purposes. A conventional commercial shopping center surrounding a parking lot is proposed between First and Second Streets, north of Main Street, which will accommodate larger users accustomed to greater vehicular activity than the specialty commercial uses fronting on Main Street.

Community serving establishments will be encouraged to locate at the intersection of Main and Second Streets where a focal point is envisioned incorporating special architectural elements and enhanced paving and special intersection treatments. At this intersection, a public plaza and park is anticipated to serve as the heart of the downtown and provide open space to accommodate outdoor events. The closure of Central Avenue between Main Street and Stimson Avenue also may be considered to enhance the proposed plaza. Other community service elements are designed to locate in close proximity to the existing civic center bounded by Stimson Avenue, Main Street and Second Street.

On the fringes of the DBD, office, residential and light industrial uses will exist. Existing office and industrial structures will be encouraged to upgrade building exteriors and landscaping as necessary to harmonize with surrounding development.

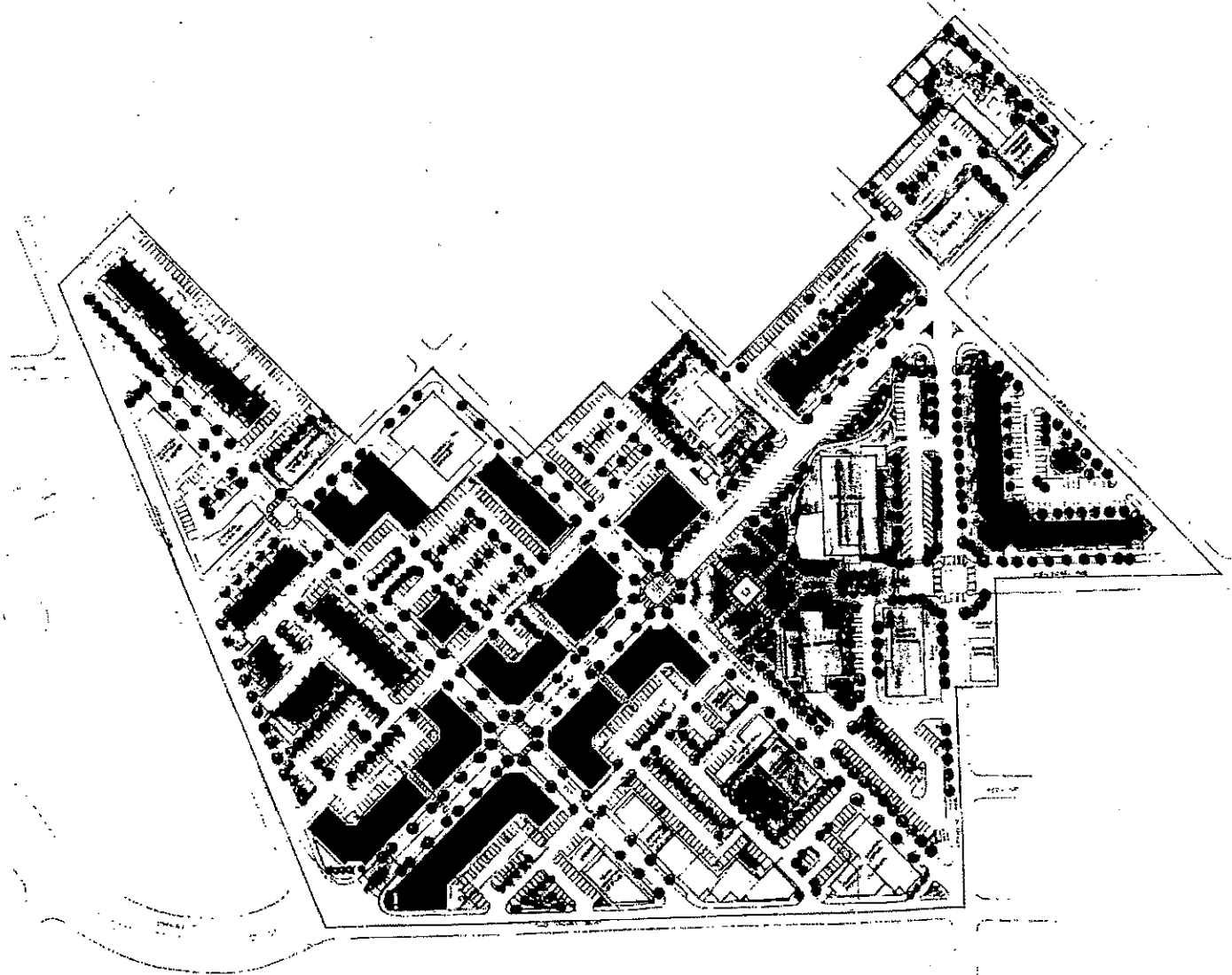
3.2 SUB AREA PLAN

The 23.7-acre Specific Plan area has been divided into fourteen (14) sub areas in order to facilitate discussion and development phasing (see Figure 6, *Land Use Plan*). The sub area boundaries have been established based on proposed land



LAND USE PLAN
FIGURE 6





This exhibit is provided for illustrative purposes only. Actual design of each parcel including ingress, egress, internal circulation and building plans shall be reviewed and approved by the City prior to demolition or substantial renovation of existing structures and/or construction of new buildings on each parcel.

ILLUSTRATED SITE PLAN
FIGURE 7



TABLE 1 – LAND USE PLAN STATISTICAL SUMMARY

SUB AREA	LAND USE	SITE AREA (acres)	MAXIMUM SQUARE FOOTAGE	TOTAL DWELLING UNITS	DENSITY (DU/AC)	PARKING PROVIDED ⁽¹⁾
1	MIXED USE: Commercial Office/Residential ⁽²⁾	0.62	20,000	- 17,300	-	60
2	MIXED-USE: Commercial Office/Residential ⁽²⁾	0.95	22,730 19,250	-	-	50
3	MIXED-USE: Commercial Office/Residential ⁽³⁾ Multi-Family Residential	2.98	22,350 18,350	- 25	- 16	182
4	MIXED-USE: Commercial Office/ Residential ⁽²⁾ Community Services	3.35	38,700 7,500 14,000	-	-	210
5	MIXED-USE: Commercial Community Services	1.80	12,000 9,800	-	-	128
6	RESIDENTIAL Multi-Family Residential	0.96		47	49	67
7	COMMUNITY SERVICE: Senior Citizen Center Community Services	1.68	10,500 7,200	-	-	73
8	OFFICE	0.56	20,400	-	-	44
9	RESIDENTIAL: Multi-Family Residential Woman's Club	1.70	9,995	36	26	97
10	RESIDENTIAL: Senior Citizen Residential	1.60		120 ⁽³⁾	75	60 ⁽³⁾
11	OFFICE	0.18	3,000	-	-	6
12	COMMUNITY SERVICE: Civic Center ⁽⁴⁾	3.82	37,680	-	-	94
13	BUSINESS PARK: Office/Light Industrial Community Services	2.80	31,900 9,200	-	-	160
14	BUSINESS PARK: Office/Light Industrial	0.70	8,850	-	-	48
TOTAL		23.70	340,725	228	-	1,279

- (1) Includes both on-street and off-street automobile parking spaces.
- (2) Office and/ or residential uses over commercial uses along Main Street.
- (3) A maximum of 75 dwelling units will be permitted if conventional multi-family residential development (and not senior citizen housing) is constructed in Sub Area 10. Approximately 106 parking spaces shall be required with conventional multi-family dwellings.
- (4) Civic Center complex includes City Hall, County Library and County Health Center.

use and location, and range in size from approximately 0.18-acre to 3.82 acres. A statistical summary of the Land Use Plan is shown in Table 1, *Land Use Summary Statistical Summary*.

3.2.1 Sub Area One

Mixed-use development consisting of commercial, office and residential uses is proposed for Sub Area One which is bounded on the north by Main Street, on the south by Old Valley Boulevard, and on the west by First Street. The southeastern portion of Sub Area One directly abuts Sub Area Fourteen. This 0.62-acre sub area allows up to 37,000 square feet of proposed building area. An interior parking lot exists which can be accessed via First Street and Old Valley Boulevard. Off-street parking should not be readily visible from Main Street, therefore ensuring the attractiveness of the primary entrance into the DBD.

Buildings along the south side of Main Street between Glendora Avenue and First Street may be occupied by specialty retail stores which should be pedestrian-oriented and complement a small town appearance. The storefronts shall be located side-by-side with no setbacks from the sidewalk in order to promote a "window shopping" ambience. A minimum building depth of 60 feet from the edge of the sidewalk is recommended for buildings fronting on Main Street to allow for a variety of retail uses. In general, retail uses should locate on the first floor of these buildings, with office and/or residential uses provided on the upper levels.

At the intersection of Main Street and Old Valley Boulevard, the northwestern corner of Sub Area One will form part of the major entry into the DBD. As such, this corner should be designed and landscaped to welcome residents and visitors into the DBD. A special architectural treatment (such as a tower or flag pole) should be incorporated into the building design facing this intersection. This street corner should be "pinched" to heighten pedestrian movement and safety, while creating an area suitable for street tree plantings.

3.2.2 Sub Area Two

Sub Area Two consisting of 0.95 acres, is located on the south side of Main Street between First and Second Streets. Mixed-use development, incorporating commercial, office and residential uses, is proposed for this area. Pedestrian-oriented, specialty retail shops will be encouraged to operate along Main Street so as to form a continuous "wall" of storefronts that recall a traditional main street shopping environment. The upper floor(s) of commercial and mixed-use structures should be targeted for use as office space and/or residential purposes. Off-street parking may be provided behind the buildings lining Main Street, and pedestrian access from the parking lot to Main Street will be facilitated by an accentuated walkway between buildings.

The intersection of Main Street and Second Street could serve as part of a focal point for the downtown. Enhanced paving and landscaping should be incorporated into the right-of-way and a public park/plaza will be created on the opposite side of Second Street in Sub Area Twelve. A special architectural feature, such as a tower or flag pole, should be designed into the building elevation at this intersection.

3.2.3 Sub Area Three

Sub Area Three consists of 2.98 acres of mixed-use development planned to include a combination of commercial, office and residential uses. Glendora Avenue and First, Main and Workman Streets form the limits of the sub area, which is envisioned to offer approximately 182 parking spaces and 40,700 square feet of building area. Multi-family residential use is proposed for the northern half of the sub area; specifically, twenty-five townhomes are proposed at a density of 16 units per acre. Development of a different housing type, such as stacked flats, apartments, or condominiums also is permitted, but must be developed under design standards to be provided by the project applicant and reviewed and approved by the City of La Puente.

Main Street should be fronted by commercial buildings accommodating specialty retail stores on the ground level and office and/or residential use on the upper stories. Retail storefronts should emanate a small town, urban atmosphere and be oriented to the pedestrian. The southwestern corner of Sub Area Three abuts the major Main Street entrance to the DBD. A special architectural feature, such as a third story element or flagpole, should be incorporated into the building design at the intersection of Main Street and Glendora Avenue and the corner should be "pinched" to heighten pedestrian movement and safety and to offer a view down Main Street.

Off-street parking will be provided in a lot accessible by way of First Street and Glendora Avenue. Pedestrian access to Main Street from the parking lot will be made available by a walkway constructed between two buildings.

3.2.4 Sub Area Four

Mixed-use development consisting of commercial, office, residential and community service uses is proposed for Sub Area Four. The sub area consists of 3.35 acres bounded by Main, First, Second and Workman Streets and is unique in that it is designed to accommodate community serving users such as a market, a pharmacy, specialty furniture stores and variety/department stores that are not dependant on pedestrian traffic for business. A restaurant and a conventional shopping center planned for larger establishments is envisioned to locate in the northern two-thirds of the sub area, facing an interior parking lot. Convenient pedestrian access to Main Street from the shopping center parking lot

will be available via an accentuated walkway positioned between two buildings.

Main Street should be lined with commercial buildings occupied by specialty retail shops and, as in Sub Area One through Three, office and residential use should be encouraged to use space on upper floors. The intersection of Main and Second Streets should be promoted as a focal point for the downtown. The building constructed on the northwest corner of the intersection should serve a community-wide or cultural function such as a theater, art or performance center and a special architectural feature, similar to those on coinciding corners should be part of the building exterior.

The existing General Telephone Business Office located on Workman and Second Streets will continue to operate at its present location. Exterior renovations or landscape improvements will be encouraged as necessary to blend the building into the traditional theme of the DBD.

3.2.5 Sub Area Five

Sub Area Five is located contiguous to Main Street, between Second and Third Streets. Development may include a mixture of commercial and community service elements which relate in size and scale to surrounding structures. This sub area consists of 1.80 acres of land including 21,800 square feet of proposed building area and 21 parking spaces. An off-street parking lot may be provided which will be directly accessible from both Main Street and Second Street.

The southwestern region of this sub area should be included as part of a focal point for the downtown. On the corner of Main and Second Streets, building design should incorporate a special architectural feature such as a third story element for accentuation. This building should be encouraged for use by a community or region serving purpose such as a large market or other general commerce enterprise which compliments, but does not compete with the specialty retail stores located further west on Main Street.

Community service use is planned for the corner of Main and Third Streets which should compliment the Civic Center complex located across Main Street. Such use is envisioned as a community recreation center or other similar institution.

3.2.6 Sub Area Six

Sub Area Six contains a total of 6 parcels on Main Street between Second Street and Stimson Avenue, and contains a total of 0.96 acres. The area's proximity to the old downtown and civic center requires that the design of projects add to, and strengthen, the visual character and identity of the area.

The Sub Area is intended to provide residential opportunities in close proximity to the DBD and to encourage residents to support the nearby shops and services.

The Sub Area is intended to provide opportunities for the development of multiple-family residential projects consisting of attached or detached dwelling units at a density of 49 dwelling units per acre.

Residential units can consist of clustered individual dwelling units, duplex dwellings, apartments, townhomes, or senior citizen housing. A condominium form of ownership may be considered for projects containing not less than three dwelling units.

The consolidation of lots is encouraged to enhance opportunities for high density residential housing development and more flexibility in project site plan design. Projects shall be designed to minimize the visual impact of enclosed and open parking spaces upon Main Street. The location, orientation, design of garage doors, and landscape buffers shall be used to reduce the visual impacts associated with vehicle parking.

3.2.6 Sub Area Seven

Sub Area Seven consists of 1.68 acres and is located significantly east of the retail stores on Main Street and in close proximity to the civic center which makes it an ideal location to develop with community service elements. This sub area is located in the easternmost portion of the Specific Plan area, north of Main Street between Albert Avenue and Fifth Street.

The existing 10,500 square foot structure at the corner of Main Street and Stimson Avenue will be preserved and renovated for use as a Senior Citizen Center. Establishment of complimentary community service uses, such as a day-care or education center with a small lawn area, should be encouraged in an adjacent building. Off-street parking will be provided behind the existing structure to serve both buildings, accessible via Main Street and Albert Avenue.

3.2.7 Sub Area Eight

Sub Area Eight, bounded by First Street, Workman Street and Glendora Avenue, is designed for office use. The existing office building, located at the intersection of First Street and Glendora Avenue, is expected to continue operating in its present location, and a second office building is proposed to be developed adjacent to Workman Street. Renovation of the existing building will be encouraged to harmonize with surrounding development and blend into the traditional character of the DBD.

Parking will be available within the sub area in an off-street lot accessible via First Street to the west. Building design and landscaping in the

parking area and around the buildings themselves should be sensitive to adjacent residential development.

3.2.8 Sub Area Nine

Sub Area Nine contains a total of 8 existing parcels containing a total of 1.70 acres. The area is directly east of First Street, between Workman and Rowland Streets.

The Sub Area is intended to provide opportunities for the development of multiple-family residential projects consisting of attached or detached dwelling units at a density of 26 dwelling units per acre.

Residential units can consist of clustered individual dwelling units, duplex dwellings, apartments, townhomes, senior citizen housing. A condominium form of ownership may be considered for projects containing not less than three dwelling units.

The consolidation of lots is encouraged to enhance opportunities for high density residential housing development and more flexibility in project site plan design.

Projects shall be designed to minimize the visual impact of enclosed and open parking spaces. The location, orientation, and design of garage doors, and landscape buffers shall be used to reduce the visual impacts associated with vehicle parking.

The existing 9,995 square foot La Puente Valley Woman's Club, located at the northeast corner of First Street and Workman Street is listed on the National Register of Historic Places. The Club was originally constructed in 1923 and is largely in its original condition. It has played a key role in the social and cultural development of the City. It is unique in its adaptation of craftsman style bungalow architecture. The retention and refurbishment of the facility is encouraged.

2.3.10 Sub Area Ten

Sub Area Ten contains a total of 7 existing parcels containing a total of 1.60 acres. The triangular area is bounded by Central Avenue, Albert Street, and Stimson Avenue.

The Sub Area is intended to provide opportunities for the development of multiple-family residential projects consisting of attached or detached dwelling units at a density of 75 dwelling units per acre. Because of the area's proximity to the County Health Center, public library, Senior Center and City Hall, senior citizen housing is encouraged.

Residential units can consist of clustered individual dwelling units, duplex dwellings, apartments, townhomes, or senior citizen housing. A

condominium form of ownership may be considered for projects containing not less than three dwelling units.

Projects shall not exceed a height of three stories or 36 feet, whichever is more restrictive.

The consolidation of lots is encouraged to enhance opportunities for high-density residential housing development and more flexibility in project site plan design.

Projects shall be designed to minimize the visual impact of enclosed and open parking spaces. The location, orientation, design of garage doors, and landscape buffers shall be used to reduce the visual impacts associated with vehicle parking.

3.2.11 Sub Area Eleven

Sub Area Eleven is the smallest to the fourteen Sub Areas, consisting of only 0.18 acres of land. This Sub Area was designed to preserve the existing 3,000 square foot office building on-site. A small parking area with six stalls exists adjacent to the building. Structural renovations and landscape intensification should be incorporated as necessary to blend the building into surrounding development and the traditional character of the DBD.

3.2.12 Sub Area Twelve

Sub Area Twelve is designated for community services and is designed to serve as the focal point for the La Puente DBD. This sub area, known as the "Civic Center", encompasses approximately 3.82 acres and is situated between Stimson Avenue and Main and Second Streets. The image of this central community service area is intended to project stability and importance as the seat of local government. The Civic Center complex represents the hub of civic, social, cultural and institutional interaction. As such, Sub Area Twelve functions as an activity generator, promoting uses which cater to the public at large.

The physical image of the major community service sub area is dominated by large-scale, civic-oriented buildings which include the La Puente City Hall, the County of Los Angeles Library and the County Health Facility. These existing structures are situated on large publicly owned parcels exhibiting large set backs surrounded by ample open space. The existing civic structures are characterized by single-story, horizontally-oriented buildings, which are compatible with surrounding land uses.

In keeping with its role, the establishment of a public plaza in Sub Area Twelve is appropriate, utilizing land available on the City Hall and County facilities properties, and through street improvements on Central Avenue. The City also may wish to consider closure of Central Avenue between

Main Street and Stimson Avenue in order to enhance the possible configurations of the public plaza. A portion of this vacated section may be modified into a landscaped cul-de-sac for dropping off visitors to the Civic Center complex. The square will serve as a gathering place and accommodate outdoor activities.

3.2.13 Sub Area Thirteen

Sub Area Thirteen contains approximately 2.80 acres. Governmental or quasi-governmental/community service uses such as a community-based police facility (up to a maximum of 9,200 square feet total) may be established in the eastern portion of Sub Area Thirteen, across Second Street from the County Public Library. A maximum of 31,900 square feet of office and light industrial uses are planned for the remaining portions of the sub area, facing Old Valley Boulevard and First Street. The existing office building in the southern portion of the sub area will be preserved and upgraded as necessary to blend into adjacent development as necessary.

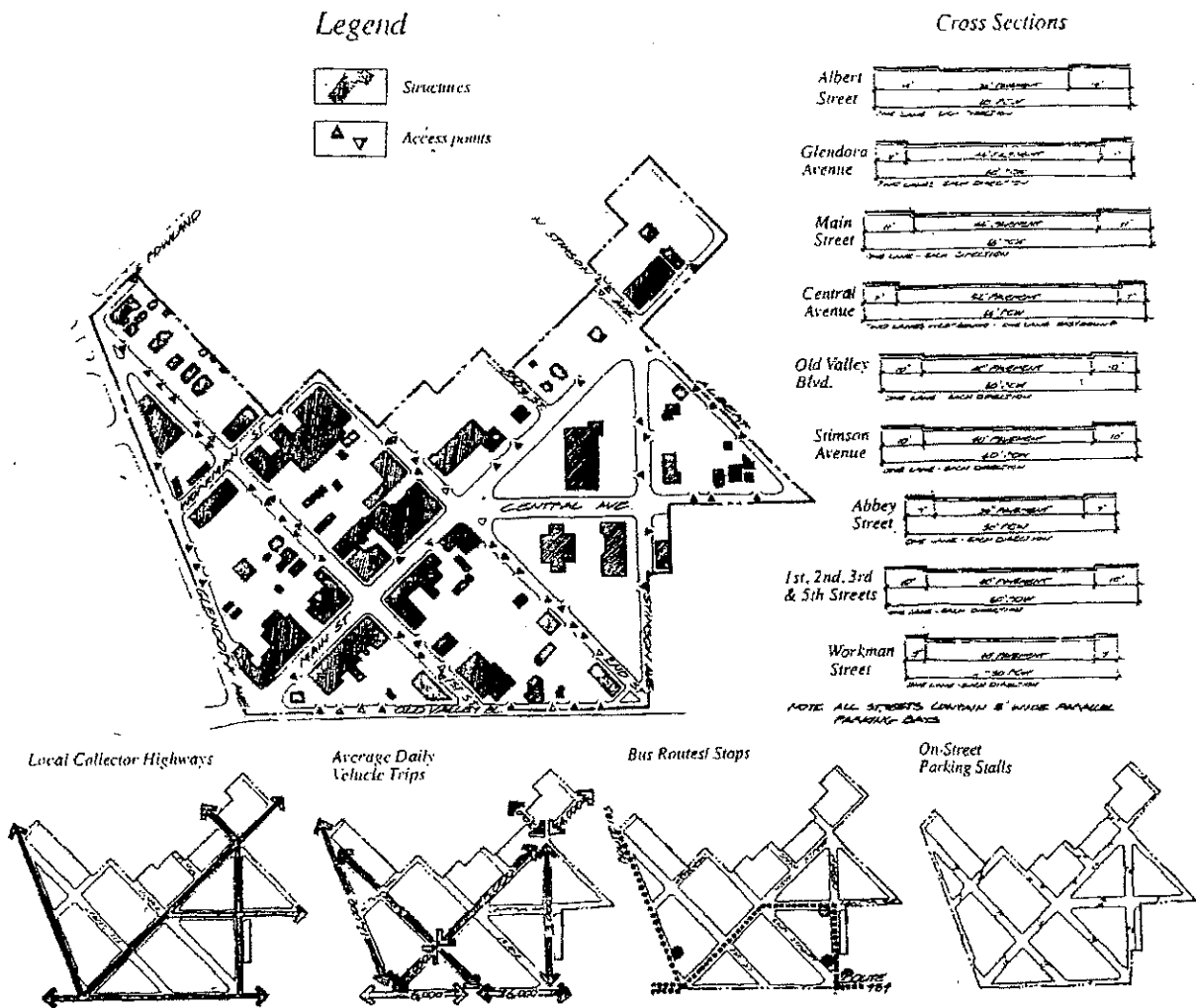
3.2.14 Sub Area Fourteen

Sub Area Fourteen is located on 0.70 acres between First Street and Old Valley Boulevard, adjacent to the Sub Area One and south of Main Street. A maximum of 8,850 square feet of office and light industrial uses shall be permitted in this sub area.

The "Fraure Building," located at the intersection of First Street and Old Valley Boulevard in the southernmost portion of Sub Area Fourteen, is potentially eligible for the National Register of Historic Places. The structure was originally constructed in 1913 and is unique in its irregular plan and fine condition for a commercial building of the early Twentieth Century. The Fraure Building is important to the preservation of historic buildings in La Puente. The Specific Plan proposes that the building be preserved and restored, as feasible and necessary.

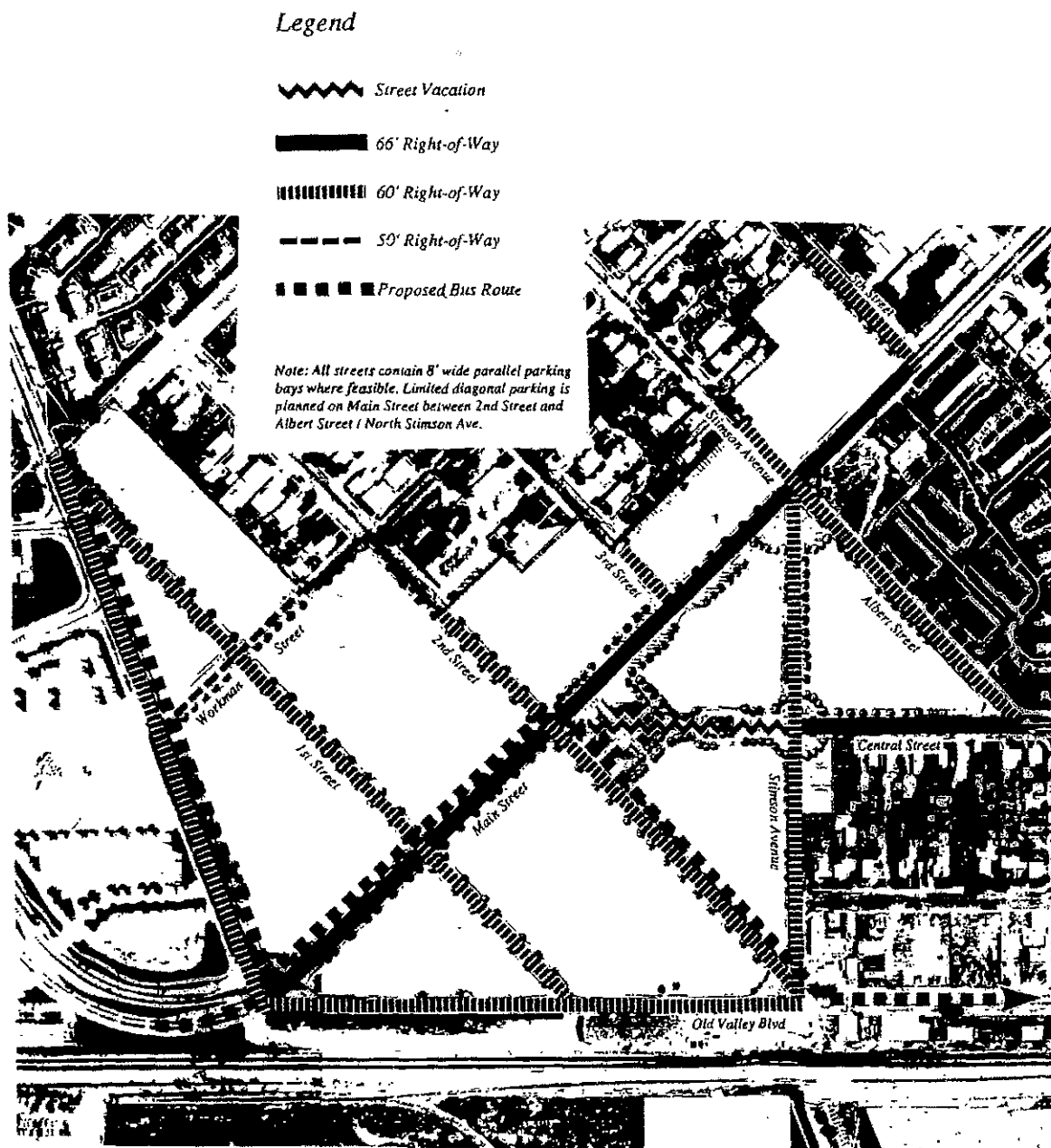
3.3 CIRCULATION PLAN

Existing circulation within the downtown is characterized by a traditional grid pattern providing access to all areas of the DBD, as depicted in Figure 8, *Existing Circulation*. Possible modifications to the types and routes of public transportation services may be implemented as deemed necessary to meet demand. The existing street system will remain in place except for minor modifications to Main Street and First Street. In addition, the closure of Central Avenue between Stimson Avenue and Main Street may be considered as a possible option for the development of a public plaza in the DBD. The proposed Circulation Plan, as depicted in Figure 9, *Circulation Plan*, is intended to enhance the streetscape image and promote pedestrian safety.



EXISTING CIRCULATION
FIGURE 8





CIRCULATION PLAN
FIGURE 9



3.3.1 Central Avenue

The Specific Plan proposes the establishment of a public plaza near the intersection of Main Street and Central Avenue. The public plaza may include street modifications to create a more pedestrian-friendly streetscape, such as enhanced paving, median plantings, landscaping and modifications to City Hall and/or County facility properties. Closure of Central Avenue between Stimson Avenue and Main Street also may be considered.

3.3.2 Main Street

The proposed street system relies on a network of newly upgraded and existing circulation features. Main Street, which serves as the central east/west corridor of the DBD, is a two lane undivided street. Proposed improvements to Main Street include "pinched" intersections to provide pedestrian safety and slower traffic, enhanced paving treatments, diagonal parking stalls and planting pockets provided at regular intervals. Figure 10, *Proposed Main Street Cross Sections*, depicts typical cross sections of the proposed Main Street improvements.

3.3.3 First Street

First Street should incorporate diagonal parking stalls and consider enhanced paving (similar in design to enhanced Main Street paving), at its intersection with Workman Street. First Street and the entire grid-oriented street pattern throughout the DBD should be reinforced with formal landscape treatments and pedestrian amenities to strengthen downtown intensification efforts.

3.4 PUBLIC FACILITIES AND INFRASTRUCTURE PLAN

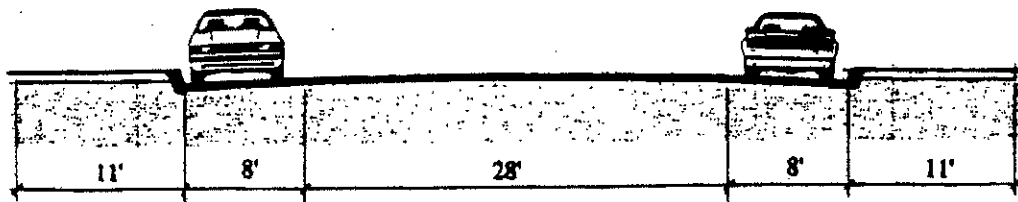
Since the La Puente DBD is already urbanized, necessary infrastructure facilities are in place. Sewer, water and drainage systems are sized to accommodate the increases in intensity of land use planned in this Specific Plan.

3.4.1 Water Service

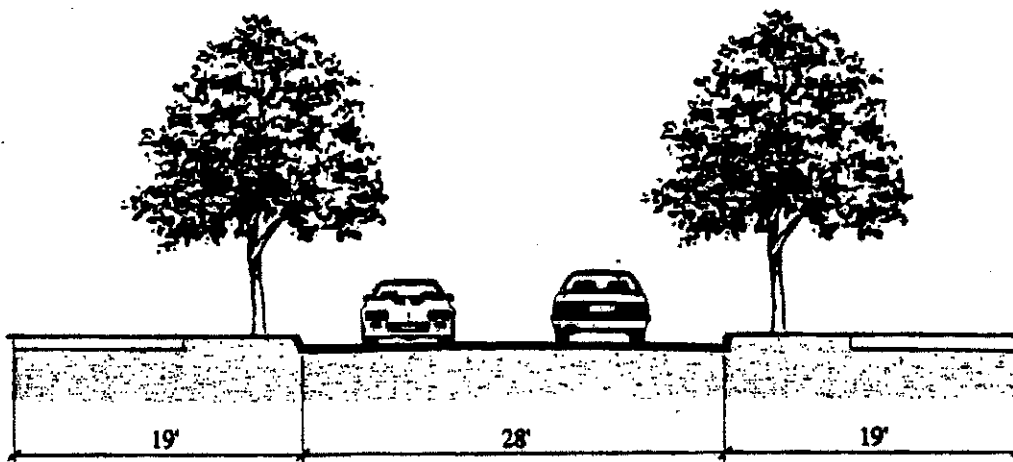
The DBD is located within the jurisdiction of the La Puente Valley County Water District. There is sufficient capacity available to service the Specific Plan area even with an increase in density and intensity of use.

This available capacity is provided by three wells located in the adjoining City of Baldwin Park. Three reservoirs (water tanks) are located approximately one-quarter mile east of the DBD on the south side of Main Street. The largest reservoir is an above ground three million gallon (MG) water storage tank. The remaining two reservoirs consist of smaller underground tanks (650,000 MG and 250,000 MG).

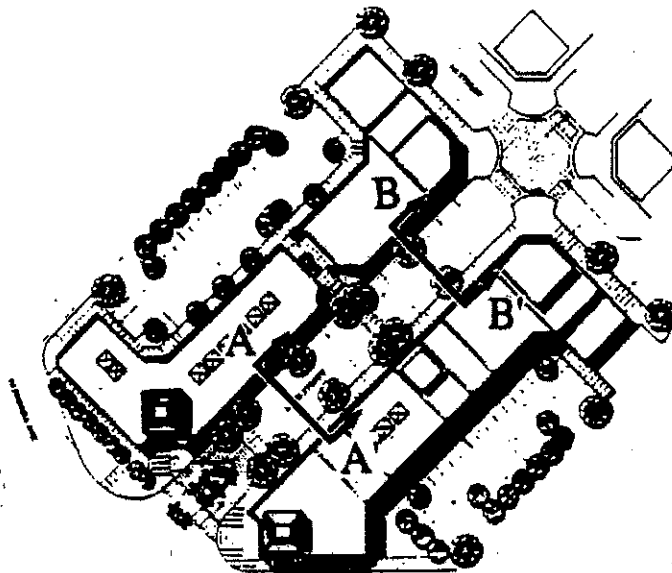
SECTION A-A'



SECTION B-B'



KEY MAP



PROPOSED MAIN STREET CROSS SECTION
FIGURE 10

At present, the backbone water system consists of a looped series of water lines within the backbone roadway system. Existing water mains located within DBD street rights-of-way are:

Glendora Avenue – A 16-inch AC line extends along the length of Glendora Avenue; its southern end terminates at Main Street/Stafford Street.

Main Street – A 16-inch AC line is located within the Main Street right-of-way and connects the Glendora Avenue water line to the reservoir to the east of the DBD on Main Street.

Central Avenue – a 6-inch AC line extends southeastward from its connection with the 10-inch AC line in Second Street.

Old Valley Boulevard – A 6-inch AC line extends from south Common Avenue to connect with a 6-inch AC line in Stimson Avenue. A 4-inch AC line, located in Old Valley Boulevard, extends from south Common Avenue to just west of the Stimson Avenue/Second Street intersection where it widens into a 10-inch AC segment before terminating about 200 feet southeast of First Street.

Stimson Avenue – A 6-inch AC line connects the Central Avenue line with the water line in Old Valley Boulevard, and a 12-inch CIC line extends from Main Street north to east Temple Avenue.

Abbey Street – An 8-inch AC line located in Abbey Street, connects to lines in Second Street and south Common Avenue.

First Street – An 8-inch AC line runs between east Rowland Street, south to Old Valley Boulevard. At Old Valley Boulevard, a 6-inch AC line connector links it with the 10-inch AC line in Second Street.

Second Street – South of Main Street, a 10-inch AC line is located in the Second Street right-of-way. This line turns west on Old Valley Boulevard to form a loop with the 8-inch AC line in First Street. The portion of the loop between Second and First Streets is a 6-inch AC line.

Fifth Street – A 6-inch AC line extends from Main Street, north to east Rowland Avenue.

Workman Street – There are no water lines located within the Workman Street right-of-way.

Albert Avenue – An 8-inch AC line connects with lines in Central Avenue and Main Street.

3.4.2 Sewer Service

The sewer system consists of a series of sewer lines located within the circulation network. Existing sewer lines located within the DBD street rights-of-way are:

Main Street - A 10-inch sewer line is located in Main Street from just east of Glendora Avenue to just east of Stimson Avenue.

Stimson Avenue - An 8-inch sewer line is located within a portion of Stimson Avenue terminating at Main Street.

Central Avenue - An 8-inch sewer line is located with Central Avenue and extends from Main Street southeast to almost Valley Boulevard.

Old Valley Boulevard - An 8-inch sewer line is located in Old Valley Boulevard. This line connects with the line in Main Street.

Glendora Avenue - An 8-inch sewer line is located within the Glendora Avenue right-of-way from its intersection with Hacienda Boulevard to Main Street.

3.5 SCHEMATIC PLAN

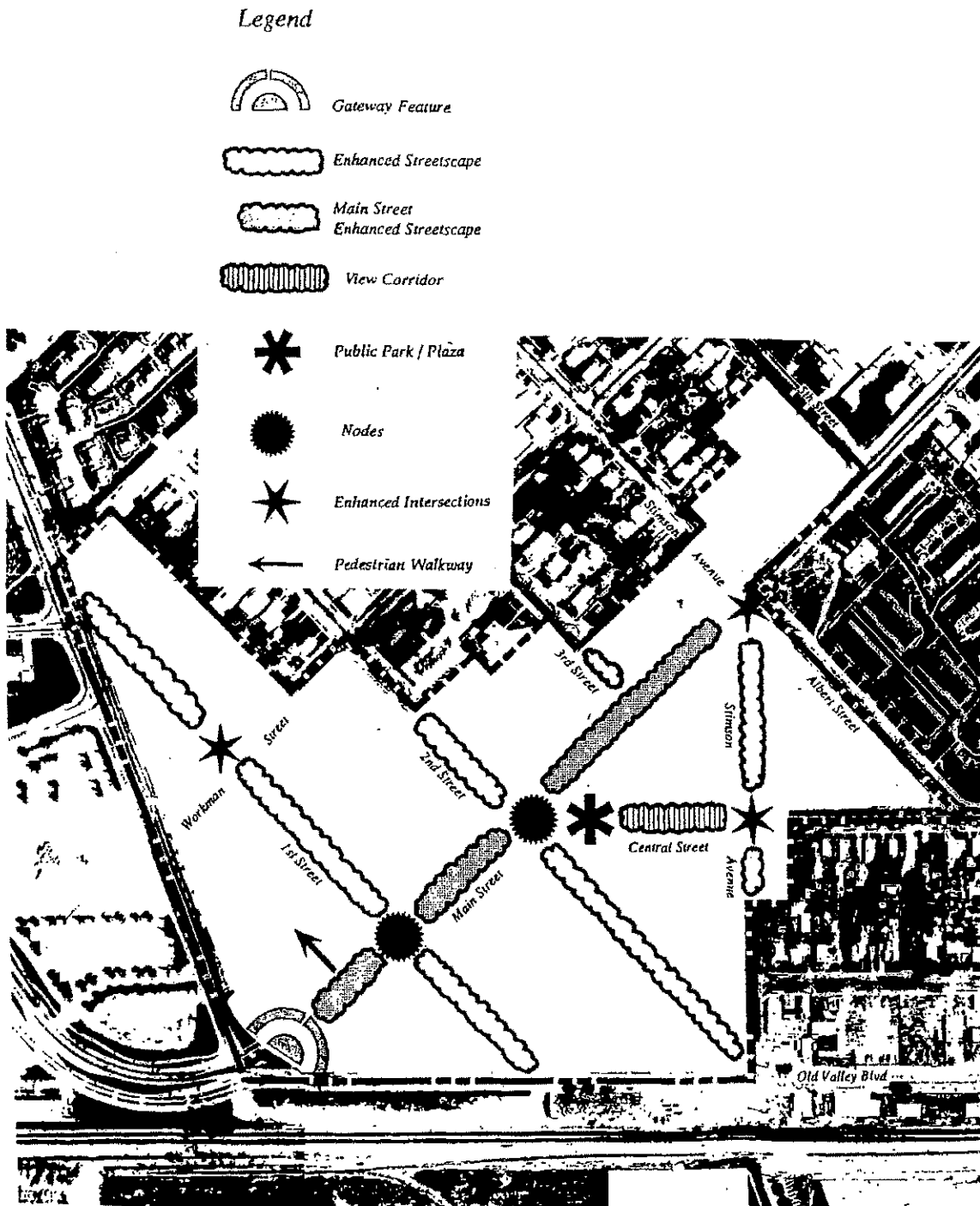
A sense of identity and image aids in the structuring of a complex environment into distinct and memorable subparts. In relation to the DBD, the degree to which a sense of image is achieved or enhanced is dependent upon how well existing image features are identified and on the proper orchestration of new features designed to increase "imageability." The conceptual design image plan for the La Puente DBD is depicted in Figure 11, *Schematic Plan*.

The maintenance and enhancement of a traditional Southern California urban center image will be implemented through the goals, objectives and policies of this Specific Plan (see Section 2.0). Perhaps the three strongest image setting influences within the DBD are: 1) The traditional image of the existing downtown built environment indicative of the small town American main street; 2) The Civic Center complex, which functions as the social, cultural, and civic hub of La Puente; and 3) The historic characteristics of downtown La Puente.

The *Schematic Plan* (see Figure 11) describes image enhancement features of the DBD as a prelude to the Landscape Architecture Plan, which provides detailed designs related to implementation of specific character evoking elements. Major DBD elements consist of:

3.5.1 Districts

Districts are medium-to-large sections of the environment, usually visualized in a two-dimensional format, which the observer mentally enters "inside of" and which are recognizable as having some common



SCHEMATIC PLAN
FIGURE 11



identifying character. Districts are typically identifiable by their mix of land uses. In relation the DBD, districts are synonymous with the fourteen sub areas.

3.5.2 Nodes

Nodes are points of intersections of streets, avenues and pedestrian circulation features. They are junctions, convergence points or elements of concentrated activity which gain their importance from intensity of use. Nodes can be the focus of a district, such as the civic center complex which provide a focus to the entire community. The concept of nodes relates directly to the concept of paths, as defined below, since junctions are typically the convergence of paths. Existing nodes within the DBD include the La Puente City Hall and the County of Los Angeles Library and Health Center. Proposed nodes are planed at the intersections of Main Street/First Street and Main Street/Second Street.

3.5.3 Enhancement Intersections

Similar to nodes, enhancement intersections will provide the DBD streetscape with variety and visual interest. Enhancement intersections may incorporate such items as interlocking pavers, stamped and/or colored concrete or similar material, bollards, special lighting fixtures and landscaping. The selected materials and design features should complement the overall design and architectural theme for the DBD. Enhanced intersections are planned at the following locations: 1) Workman Street/First Street; 2) Main Street/Stimson Avenue; and 3) Stimson Avenue/Central Street.

3.5.4 Paths

Paths are the routes along which people and vehicles travel. Paths consist of formal vehicular streets and informal circulation features such as pedestrian paseos. Paths are elements which lead people through the environment and let the observer experience and relate to other environmental elements and activities. Existing major paths associated with the DBD include Main Street and First Street. Proposed paths include the planed park/plaza pedestrian walkways and various pedestrian paseos.

3.5.5 Gateway Feature

A gateway feature which announces entry or exit into the DBD is planned at the junction of Glendora Avenue and Old Valley Boulevard with Main Street. The gateway feature is designed to signal the user that he or she is entering downtown La Puente.

3.5.6 View Corridors

Historically, view axes tend to terminate at an important landmark (either natural or built) thus drawing attention to the prominence of the landmark as one views the terminus along a axis continuum which frames the view and heightens the sense of arrival. The predominant view corridor proposed for the DBD is Central Avenue terminating at the public park/plaza and offering views of the Main Street/Second Street building facades.

4. DEVELOPMENT REGULATIONS

4.1 GENERAL PROVISIONS

4.1.1 Purpose and Intent

This section contains the development regulations for all new construction and redevelopment with the La Puente DBD. The provisions herein will implement the City of La Puente's General Plan and are tailored to accomplish the goals, objectives and policies specified in Section 2.0 of this Specific Plan. The intent of these regulations is to establish a distinctive, well-designed and high quality environment that enhances the existing development with the DBD and maintains the small town character.

4.1.2 Relationship to the La Puente Zoning Ordinance

This Specific Plan augments the development regulations and standards of the City of La Puente Zoning Ordinance. Because of changing market, socioeconomic and political conditions, it is neither possible nor desirable for all issues, conditions or situations that may arise at some time in the future to be adequately addressed in this Specific Plan. Therefore, when an issue, condition or situation occurs which is not discussed in this Specific Plan, the regulations of the City of La Puente Zoning Ordinance that are most applicable to the issue, condition or situation shall apply. In the event that the provisions of the Specific Plan are in conflict with the City of La Puente Zoning Ordinance, the provisions of this Specific Plan shall prevail. The development regulations and standards shall apply to the DBD only and do not apply to any other area within the city limits of La Puente.

4.1.3 Interpretation

The Community Development Director or his/her designee, shall have the responsibility to interpret the provisions of the Specific Plan. All such interpretations shall be in written form and permanently maintained. Any person aggrieved by such an interpretation may appeal the decision to the Planning Commission for final determination.

4.1.4 Enforcement

The Community Development Director shall enforce the provisions of this Specific Plan and all applicable codes of all governmental agencies and jurisdictions in such matters including, but not limited to, building, mechanical, fire, and electrical codes, and codes pertaining to drainage, waste water, public utilities, and subdivision.

4.1.5 Amendments

The procedures to amend this Specific Plan shall be the same as an amendment to the City's Zoning Ordinance (see Chapter 10.80 in Title 10, City of La Puente Zoning Ordinance, in the City's Municipal Code). In addition, amendment to any element of the City of La Puente General Plan may be required if conflict is found to exist with any proposed specific plan amendment.

4.1.6 Variances

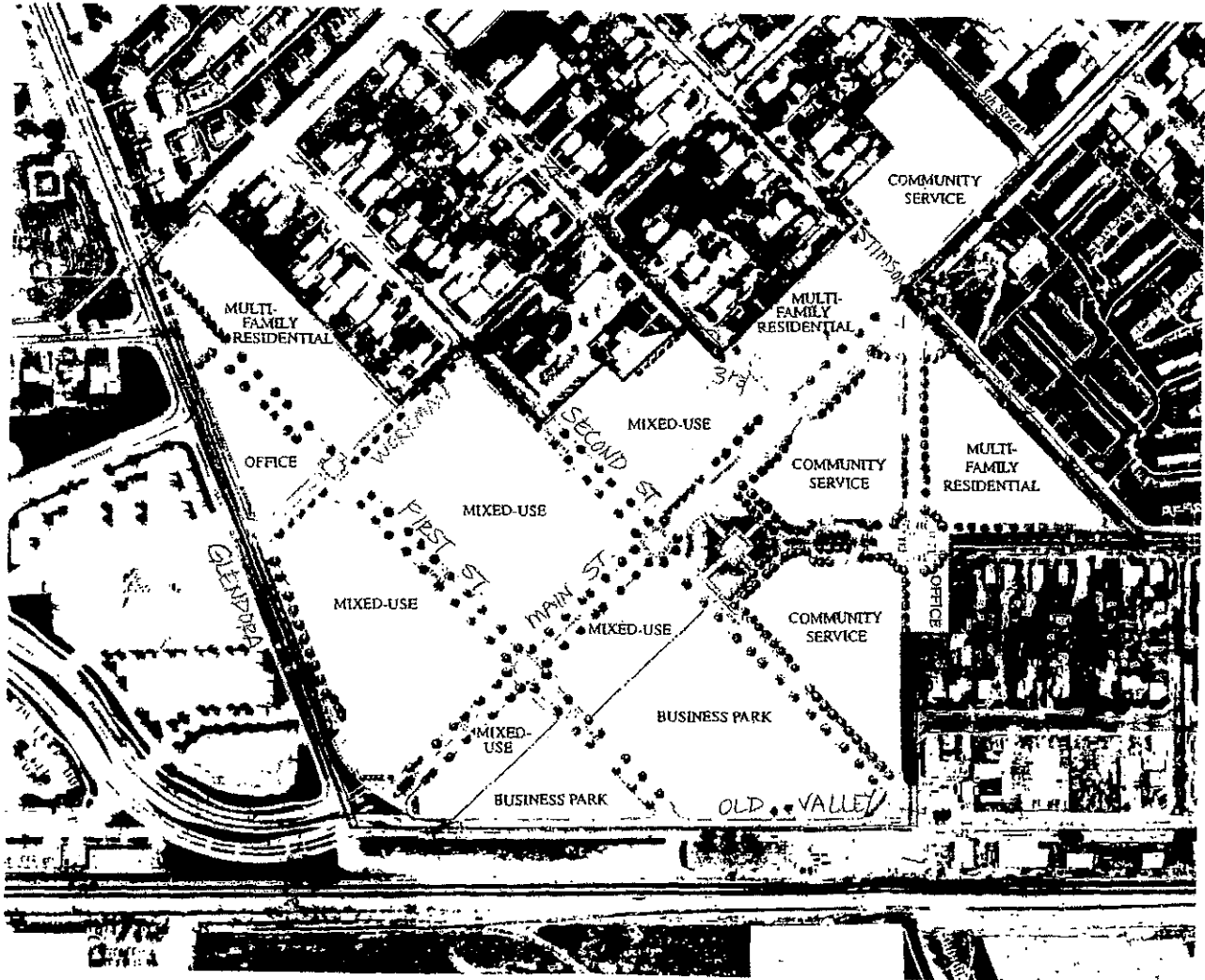
The procedures and requirements for variances shall be the same as required in the Chapter 10.80 of Title 10, City of La Puente Zoning Ordinance, in the City's Municipal Code.

4.1.7 Definitions

Words, phrases and terms not specifically defined herein shall have the same definition as provided in the Chapter 10.08 of Title 10, City of La Puente Zoning Ordinance, in the City's Municipal Code.

4.1.8 Districts

The La Puente Downtown Business District Specific Plan designates five separate and distinct districts within the DBD: the Mixed-Use District, the Office District, the Business Park District, the Community Service District, and the Multi-Family Residential District. The approximate location and size of each district is delineated on Figure 12, *Zoning Map*, on the following page.



ZONING MAP
FIGURE 12



4.2 MIXED-USE DISTRICT LAND USE REGULATIONS

4.2.1 Primary Function/Location

The Mixed-Use District is located on either side of Main Street between Glendora Avenue and Third Street in Sub Areas One, Two, Three, Four and Five. The Mixed-Use District is symbolic of many small town American main streets. The buildings are typically located side-by-side with little if any space between buildings and the building facades typically abut the sidewalk with no front yard setbacks. Thus, a continuous street wall or outdoor "room" effect is created which frames the streetscape.

The primary function of the Mixed-Use District is to promote commerce by:

- Providing specialty-oriented retail land uses designed to encourage pedestrian patronage;
- Encouraging the preservation and re-use, and redevelopment of significant existing structures and areas; and
- Promoting the development of new uses.

The intent of the Mixed-Use District is to promote uses which stimulate activity, enhance the street-life in the DBD, and promote the small town heritage and image of La Puente.

4.2.2 Permitted and Conditional Uses

The following matrix establishes the uses which are permitted, conditionally permitted or not permitted in the Mixed-Use District.

LEGEND P=Permitted Uses, C=Conditionally Permitted Uses, Uses not listed as "P" or "C" are not permitted.

MIXED-USE DISTRICT PERMITTED USES	
LAND USE	CATEGORY
Administrative, business and professional offices	P
Antique shops	P
Apparel stores	P
Art and artist's supply stores	P
Art galleries and stores selling objects of art	P
Artist and photo studios	P
Automotive parts stores	P
Bakeries, baking of items for sale on premises only	P
Barber shops and beauty shops	P

MIXED-USE DISTRICT PERMITTED USES	
LAND USE	CATEGORY
Bicycle shops	P
Blueprint and photocopy services	P
Book, gift and stationary stores	P
Business and office services	P
Camera and photography shops and studios	P
Candy stores and confectioneries	P
Catering services	P
China and glassware stores	P
Cigar stores and smoke shops	P
Cinemas, general audience (no drive-in theaters permitted)	C
Cocktail lounges	C
Compact disc/record stores	P
Convenience stores	P
Delicatessens and specialty food stores	P
Dwelling units including apartments, condominiums, and single-room occupancy units above the first floor of a commercial use, subject to development standards set forth in Sub-section 4.6.5	C
Financial institutions, including banks, savings and loan associations, finance companies, and credit unions (no drive-thru facilities)	P
Florists	P
Furniture stores	P
Gift shops and greeting card shops	P
Heating and ventilating shops, not including sheet metal fabrication	P
Hobby shops	P
Home Occupations	C
Hotels, motels and motor inns	C
Household appliance sales and repair shops	P
Ice cream stores and soda fountains	P
Interior decorating shops	P
Janitorial services and supplies	P
Jewelry stores	P
Leather goods and luggage stores	P
Liquor stores	C
Markets, including food, produce, fruit, vegetable, fish and poultry and meat markets (not including slaughtering)	P
Messenger services	P
Music and dance studios	P

❖ *City of La Puente Downtown Business District Specific Plan* ❖

MIXED-USE DISTRICT PERMITTED USES	
LAND USE	CATEGORY
Music stores	P
Musical instrument repair shops	P
Newsstand	C
Nurseries and garden supply stores with 750 square feet or less of outdoor storage area	P
Nurseries and garden supply stores with 751 square feet or more of outdoor storage area	C
Office and business machine stores, including computer sales, service and rentals	P
Optician and optometry shops	P
Outdoor sales and storage in conjunction with a primary permitted use	C
Outdoor vendors	C
Paint, glass and wallpaper stores (no outdoor storage permitted)	P
Pet and bird stores	P
Photographic developing and printing	P
Pinball and electronic games arcades	C
Post office	P
Postal annex, private ownership	P
Printing, publishing, lithography and engraving	P
Radio, stereo, and television sales and repair	P
Realtors and real estate offices	P
Restaurants, including types with outdoor dining patios/terraces or walk-up service, but with no outdoor drive-in or drive-through service	P
Restaurants, cocktail lounges, or other establishments providing live entertainment or serving alcoholic beverages	C
Secretarial services	P
Shoe stores and shoe repair shops	P
Sporting goods stores	P
Stationery stores	P
Tailor shops	P
Theaters and auditoriums within buildings	C
Toy stores	P
Travel agencies and bureaus	P
Typewriter sales and service establishments	P
Variety stores	P
Video stores	P
Watch and clock repair shops	P
Yardage goods stores (fabrics)	P

4.2.3 Additional Permitted Uses

Any use that is not specifically listed in Subsection 4.2.2, above, may be considered a permitted use, provided that the Community Development Director finds that the proposed use is substantially the same in character and intensity as those listed in Subsection 4.2.2.

4.2.4 Permitted Accessory Uses

Any use that is not specifically listed in Subsection 4.2.2, above, may be considered a permitted accessory use provided that the Community Development Director finds that the proposed accessory use is substantially the same in character and intensity as those listed in Subsection 4.2.2. Such as use is subject to the permit process which governs the category in which it falls.

4.2.5 Site Development Standards

The following matrix contains standards related to development intensity, building setbacks, and building height limitations within the Mixed-Use District.

MIXED-USE DISTRICT GENERAL DEVELOPMENT CRITERIA	
Minimum Site Area for New Development	Not Applicable
Lot Coverage	100%
Height Limitations: Maximum Stories	45 FT/3 Stories
Height Limitations: Towers, Campaniles, Rotundas Adjacent to Activity Nodes and Gateways	55 FT
Minimum Landscaping Required within Public ROW (i.e., sidewalk)	One (1) 24-inch Box Tree Per 50 Linear FT of Street Frontage

MIXED-USE DISTRICT GENERAL DEVELOPMENT CRITERIA	
BUILDING SETBACKS	
Front & Streetside Yards (as measured from the street right-of-way line):	
Main Street	10 FT
Glendora Avenue	10 FT
First Street	10 FT
Second Street	10 FT
Third Street	10 FT
Interior Side Yard	0 FT
Rear Yard	12 FT Minimum, unless a reduced yard is necessary to accommodate required on-site parking, circulation aisles, and dedication of alley R.O.W. Rear yard reduction must be processed in accordance with Section 4.1.6 in this Specific Plan

4.2.6 Outdoor Dining

A. *Purpose and Intent*

The purpose of this section is to encourage outdoor dining facilities to add to the vitality of the Downtown Business District streetscape and the urban appeal of district commensurate with a pedestrian-oriented commercial environment.

B. *General Requirements*

1. In addition to the issuance of a permit as specified below, the following requirements shall apply to all outdoor dining facilities located on public sidewalks:
 - a. Outdoor dining/seating on public sidewalks shall not encroach in front of other businesses, whether or not such adjacent business has granted permission to do so.
 - b. A clear, unobstructed pedestrian pathway a minimum of five (5) feet wide shall be maintained at all times on the public sidewalk in front of the property.
 - c. No more than four (4) tables and eight (8) chairs shall be permitted on the public sidewalk in front of the respective business for which the tables and chairs are provided. Table tops shall not exceed four (4) square feet in area.

ADDED BY 410 07-006

- d. Outdoor furniture, umbrellas, and fixtures may not contain advertising.
 - e. The serving and consumption of alcoholic beverages shall be prohibited.
2. In addition to the issuance of a permit as specified below, outdoor dining facilities on private property that are no greater than a total of 400 square feet, may be permitted without additional on-site parking or parking in-lieu or assessment fees. However, outdoor dining areas above and beyond this limit shall be subject to any such requirements as applicable.
3. The following requirements shall apply to all outdoor dining facilities:
- a. Outdoor dining/seating shall not obstruct entrances and driveways.
 - b. Decorative lighting is required for outdoor dining areas used during the evening. Lights mounted on a building shall not cause direct glare or other visual obstruction to pedestrians or motorists along the street and public walkway.
 - c. Outdoor furniture must be built of decorative, durable materials commensurate with the historical character of the District and suitable for outdoor use. Table and chair frames shall be constructed of materials consistent with the early Twentieth Century such as wrought iron, painted steel, stone and full grain hardwood. Table surfaces must be of ornamental materials such as stone, tile mosaic, heavy laminate, metal or safety glass. Outdoor furniture shall convey quality, durability and historic suitability for the District. Beach chairs, plastic chairs and tables, common folding chairs and card tables are examples of furniture that are not acceptable.
 - d. Outdoor furniture must use colors that complement and/or match the colors used on the adjacent building. Highly reflective surfaces such as stainless steel and chrome are not allowed. Colors shall be black, burgundy, green, earth tones or other suitable colors that are approved by the City.
 - e. Outdoor dining areas shall be continuously supervised by management or employees.

ADDED BY 440 02-2006

- f. The custodial maintenance of outdoor dining areas, whether on private property or public sidewalk, shall be the responsibility of the owner of the business. The cleaning of trash, debris, litter, and spills on adjacent public sidewalks or parking lots areas resulting from the business and/or its patrons shall be the responsibility of the business owner.
- g. The outdoor dining hours of operation may not extend beyond the hours of operation of the associated food or beverage establishment.
- h. All plans and permits for the outdoor dining area approved by the City must be kept on the premises for public inspection at all times during which the associated establishment is open for business.
- i. Outdoor dining areas shall be operated in a manner that meets all requirements of the Health Department of Los Angeles County and all other applicable regulations, laws, ordinances, and standards. Food establishments serving alcoholic beverages shall also obtain all necessary permits required by the State Department of Alcoholic Beverage Control.
- j. Reasonable conditions of approval to protect the public health, safety and welfare and to assure that outside dining areas comply with all applicable City Codes may be imposed by the City.

3. *Permit Required*

- a. Prior to the establishment of any outdoor dining areas, a permit for which there may be a processing fee established by City Council resolution, must be obtained from the City. Permits for outdoor dining areas in the public right-of-way shall constitute an encroachment permit for which insurance as required by the City's Risk Manager must be provided by the applicant.

In addition to a permit application form provided by the Community Development Department, the applicant shall submit a detailed plot plan, drawn to scale, including the full width of the sidewalk if a dining area on a public sidewalk is proposed, demarcating the area to be used for outdoor dining and the number and

908-70 City of LA

location of the tables and chairs. The Community Development Department may approve the permit subject to reasonable conditions of approval to protect the public health, safety and welfare and assure compliance with all applicable City Codes.

- b. The Community Development Director may approve minor permits for outdoor dining facilities with not more than four (4) tables or eight (8) tables on a public sidewalk and outdoor dining facilities that do exceed a total of 400 square feet. Larger facilities on private property shall require the approval of the Planning Commission.
- c. An outdoor dining permit may be revoked or a renewal may be denied for violation of applicable terms and codes or if the outdoor dining area otherwise poses a public nuisance.
- d. The City reserves the right to temporarily suspend permits issued for dining on public sidewalks upon reasonable notice because of anticipated or actual conflicts in the use of sidewalk areas due to street repairs, parades, festivals, emergencies, and other similar events."

APPROVED BY ORD 02-500

4.3 OFFICE DISTRICT LAND USE REGULATIONS

4.3.1 Primary Function/Location

The Office District is located between Glendora Avenue and First Street, south of Nelson Avenue and north of Workman Street (i.e., Sub Areas Eight and Eleven). The Office District is intended to provide areas for quiet, clean office-related uses that are compatible with nearby residential development. The primary function of the Office District is to promote office and professional uses in the DBD.

4.3.2 Permitted and Conditional Uses

The following matrix establishes the uses which are permitted, conditionally permitted or not permitted in the Office District:

LEGEND P=Permitted Uses, C=Conditionally Permitted Uses, Uses not listed as "P" or "C" are not permitted.

OFFICE DISTRICT PERMITTED USES	
LAND USE	CATEGORY
Administrative, business and professional offices	P
Antique shops	P
Art and artist's supply stores	P
Art galleries and stores selling objects of art	P
Artist and photo studios	P
Barber shops and beauty shops	P
Blueprint and photocopy services	P
Book, gift and stationary stores	P
Business and office services	P
Camera and photography shops and studios	P
Cigar stores and smoke shops	P
Delicatessens and specialty food stores	P
Financial institutions, including banks, savings and loan associations, finance companies, and credit unions (no drive-thru facilities)	P
Florists	P
Gift shops and greeting card shops	P
Messenger services	P
Newsstand	C
Optician and optometric shops	P
Photographic developing and printing	P
Postal annex, private ownership	P
Realtors and real estate offices	P

OFFICE DISTRICT PERMITTED USES	
LAND USE	CATEGORY
Restaurants, including types with outdoor dining patios/terraces or walk-up service, but with no outdoor drive-in or drive-through service	P
Restaurants, cocktail lounges, or other establishments providing live entertainment or serving alcoholic beverages	C
Secretarial services	P
Stationery stores	P
Tailor shops	P
Travel agencies and bureaus	P

4.3.3 Additional Permitted Uses

Any use that is not specifically listed in Subsection 4.3.2, above, may be considered a permitted use, provided that the Community Development Director finds that the proposed use is substantially the same in character and intensity as those listed in Subsection 4.3.2.

4.3.4 Permitted Accessory Uses

Any use that is not specifically listed in Subsection 4.3.2, above, may be considered a permitted accessory use provided that the Community Development Director finds that the proposed accessory use is substantially the same in character and intensity as those listed in Subsection 4.3.2. Such as use is subject to the permit process which governs the category in which it falls.

4.3.5 Site Development Standards

The following matrix contains standards related to development intensity, building setbacks, and building height limitations within the Office District.

OFFICE DISTRICT GENERAL DEVELOPMENT CRITERIA	
Minimum Site Area for New Development	Not Applicable
Lot Coverage	100%
Height Limitations: Maximum Stories	36 FT/2 Stories
Height Limitations: Towers, Campaniles, Rotundas Adjacent to Activity Nodes and Gateways	40 FT
Minimum Landscaping Required within Public ROW (i.e., sidewalk)	One (1) 24-inch Box Tree Per 50 Linear FT of Street Frontage

OFFICE DISTRICT GENERAL DEVELOPMENT CRITERIA	
BUILDING SETBACKS	
Front & Streetside Yards (as measured from the street right-of-way line):	
Glendora Avenue	10 FT
First Street	10 FT
Workman Street	10 FT
Stimson Avenue	10 FT
Central Avenue	20 FT
Interior Side Yard	5 FT; provided that 10 FT shall be required on side yards which abut residential uses.
Rear Yard	10 FT Minimum, unless a reduced yard is necessary to accommodate required on-site parking, circulation aisles, and dedication of alley R.O.W. Rear yard reduction must be processed in accordance with Section 4.1.6 in this Specific Plan

4.4 BUSINESS PARK LAND USE REGULATIONS

4.4.1 Primary Function/Location

The Business Park District is limited to Sub Areas Thirteen and Fourteen in the DBD and is designed to contain both office and light industrial uses.

4.4.2 Permitted and Conditional Uses

The following matrix contains standards related to development intensity, building setbacks, and building height limitations within the Business Park District:

LEGEND P=Permitted Uses, C=Conditionally Permitted Uses, Uses not listed as "P" or "C" are not permitted.

BUSINESS PARK DISTRICT PERMITTED USES	
LAND USE	CATEGORY
Administrative, business and professional offices	P
Antique shops	P
Art and artist's supply stores	P
Art galleries and stores selling objects of art	P
Artist and photo studios	P
Automotive parts and supply stores (no outdoor storage permitted)	P
Automotive repair (must be conducted wholly within an enclosed building; no outdoor storage permitted)	P
Barber shops and beauty shops	P
Battery rebuilding	P
Blueprint and photocopy services	P
Boat building, except ship building	P
Book, gift and stationary stores	P
Business and office services	P
Camera and photography shops and studios	P
Cigar stores and smoke shops	P
Cosmetics manufacture	P
Electric or neon sign manufacturing	P
Financial institutions, including banks, savings and loan associations, finance companies, and credit unions (no drive-through facilities)	P
Florists	P
Food products manufacturing, storage and processing	P
Fruit and vegetable canning, preserving and freezing	P
Garment manufacture	P

BUSINESS PARK DISTRICT PERMITTED USES	
LAND USE	CATEGORY
Gift shops and greeting card shops	P
Health clubs and fitness centers	P
Laundries	P
Messenger services	P
Newsstand	P
Optician and optometry shops	P
Photographic developing and printing	P
Postal annex, private ownership	P
Realtors and real estate offices	P
Restaurants, including types with outdoor dining patios/ terraces or walk-up service, but with no outdoor drive-in or drive-through service	C
Restaurants, cocktail lounges, or other establishments providing live entertainment or serving alcoholic beverages	C
Secretarial services	P
Sheet metal shops	P
Stationery stores	P
Tailor shops	P
Textile manufacture	P
Tile, manufacture of wall and floor tile and related small tile	P
Travel agencies and bureaus	P
Wholesale business, storage buildings and warehouses	P

4.4.3 Additional Permitted Uses

Any use that is not specifically listed in Subsection 4.4.2, above, may be considered a permitted use, provided that the Community Development Director finds that the proposed use is substantially the same in character and intensity as those listed in Subsection 4.4.2.

4.4.4 Permitted Accessory Uses

Any use that is not specifically listed in Subsection 4.4.2, above, may be considered a permitted accessory use provided that the Community Development Director finds that the proposed accessory use is substantially the same in character and intensity as those listed in Subsection 4.4.2. Such as use is subject to the permit process which governs the category in which it falls.

4.4.5 Site Development Standards

The following matrix contains standards related to development intensity, building setbacks, and building height limitations within the Business Park District.

BUSINESS PARK DISTRICT GENERAL DEVELOPMENT CRITERIA	
Minimum Site Area for New Development	Not Applicable
Lot Coverage	100%
Height Limitations: Maximum Stories	36 FT/2 Stories
Height Limitations: Towers, Campaniles, Rotundas Adjacent to Activity Nodes and Gateways	40 FT
Minimum Landscaping Required within Public ROW (i.e., sidewalk)	One (1) 24-inch Box Tree Per 50 Linear FT of Street Frontage

BUSINESS PARK DISTRICT GENERAL DEVELOPMENT CRITERIA	
BUILDING SETBACKS	
Front & Streetside Yards (as measured from the street right-of-way line):	
First Street	10 FT
Second Street	15 FT
Old Valley Boulevard	8 FT
Interior Side Yard	5 FT
Rear Yard	10 FT Minimum, unless a reduced yard is necessary to accommodate required on-site parking, circulation aisles, and dedication of alley R.O.W. Rear yard reduction must be processed in accordance with Section 4.1.6 in this Specific Plan

4.5 COMMUNITY SERVICE DISTRICT LAND USE REGULATIONS

4.5.1 Primary Function/Location

The Community Service District is limited to Sub Areas Seven and Twelve in the DBD and is designed to contain community-oriented land uses which provide public or quasi-public services to the community at large or a segment of the community. Limited commercial and office uses are also permitted within the Community Service District.

4.5.2 Permitted and Conditional Uses

The following matrix contains standards related to development intensity, building setbacks, and building height limitations within the Community Service District:

LEGEND A=Administrative Review, P=Permitted Uses, C=Conditionally Permitted Uses, Uses not listed as "P" or "C" are not permitted.

COMMUNITY SERVICE DISTRICT PERMITTED USES	
LAND USE	CATEGORY
Administrative, business and professional offices	P
Art galleries and stores selling objects of art	P
Barber shops and beauty shops	P
Blueprint and photocopy services	P
Book, gift and stationary stores	P
Business and office services	P
Child care centers	C
Churches	C
Delicatessens and specialty food stores	P
Drug stores and pharmacies	P
Fire station	C
Financial institutions, including banks, savings and loan associations, finance companies, and credit unions (no drive-through facilities)	P
Gift shops and greeting card shops	P
Governmental offices and quasi-governmental	C
Health clubs and fitness centers	P
Home occupations	A
Hotels, motels and motor inns	C
Library	C
Medical and dental offices and clinics	P
Music and dance studios	P
Office and business machine stores, including computer sales, service and rentals	P

COMMUNITY SERVICE DISTRICT PERMITTED USES	
LAND USE	CATEGORY
Optician and optometry shops	P
Photographic developing and printing	P
Printing, publishing, lithography and engraving	P
Private and public clubs	C
Public utility of public service structures and installations	C
Postal annex, private ownership	P
Realtors and real estate offices	P
Restaurants, including types with outdoor dining patios/ terraces or walk-up service, but with no outdoor drive-in or drive-through service	C
Restaurants, cocktail lounges, or other establishments providing live entertainment or serving alcoholic beverages	C
Schools, private (trade, vocation)	C
Secretarial services	P
Senior citizen center & senior daycare facility	P
Stationery stores	P
Theaters and auditoriums within buildings	C
Travel agencies and bureaus	P
Wholesale business, storage buildings and warehouses	P

4.5.3 Additional Permitted Uses

Any use that is not specifically listed in Subsection 4.5.2, above, may be considered a permitted use, provided that the Community Development Director finds that the proposed use is substantially the same in character and intensity as those listed in Subsection 4.5.2.

4.4.3 Permitted Accessory Uses

Any use that is not specifically listed in Subsection 4.5.2, above, may be considered a permitted accessory use provided that the Community Development Director finds that the proposed accessory use is substantially the same in character and intensity as those listed in Subsection 4.5.2. Such as use is subject to the permit process which governs the category in which it falls.

4.4.5 Site Development Standards

The following matrix contains standards related to development intensity, building setbacks, and building height limitations within the Community Service District.

COMMUNITY SERVICE DISTRICT GENERAL DEVELOPMENT CRITERIA	
Minimum Site Area for New Development	Not Applicable
Lot Coverage	100%
Height Limitations: Maximum Stories	36FT/2 Stories
Height Limitations: Towers, Campaniles, Rotundas Adjacent to Activity Nodes and Gateways	45 FT
Minimum Landscaping Required within Public ROW (i.e., sidewalk)	One (1) 24-inch Box Tree Per 50 Linear FT of Street Frontage

COMMUNITY SERVICE DISTRICT GENERAL DEVELOPMENT CRITERIA	
BUILDING SETBACKS	
Front & Streetside Yards (as measured from the street right-of-way line):	
Main Street (north side)	10 FT
Main Street (south side)	30 FT
Second Street	20 FT
Fifth Street	20 FT
Stimson Avenue	30 FT
Interior Side Yard	10 FT
Rear Yard	15 FT Minimum, unless a reduced yard is necessary to accommodate required on-site parking, circulation aisles, and dedication of alley R.O.W. Rear yard reduction must be processed in accordance with Section 4.1.6 in this Specific Plan

4.6 MULTI-FAMILY RESIDENTIAL DISTRICT LAND USE REGULATIONS

4.6.1 Primary Function/Location

The Multi-Family Residential District shall apply to Sub Areas 6,9 and 10 in the La Puente DBD.. The purpose of this district is to provide higher density residential uses in close proximity to retail and office uses within the DBD.

4.6.2 Permitted and Conditional Uses

The following matrix contains standards related to development intensity, building setbacks, and building height limitations within the Multi-Family Residential District:

LEGEND A=Administrative Review, P=Permitted Uses, C=Conditionally Permitted Uses, Uses not listed as "P" or "C" are not permitted.

MULTI-FAMILY RESIDENTIAL DISTRICT PERMITTED USES	
LAND USE	CATEGORY
Multiple family dwelling units, apartments, and condominiums	P
Boarding and rooming houses	P
Day-care facilities	P
Group homes	P
24-Hour foster care homes	P
Resthomes	P
Home occupations	A
Senior citizen housing	C
Single room occupancy facilities	C

4.6.3 Additional Permitted Uses

Any use that is not specifically listed in Subsection 4.6.2, above, may be considered a permitted use, provided that the Community Development Director finds that the proposed use is substantially the same in character and intensity as those listed in Subsection 4.6.2.

4.6.4 Permitted Accessory Uses

Any use that is not specifically listed in Subsection 4.6.2, above, may be considered a permitted accessory use provided that the Community Development Director finds that the proposed accessory use is substantially the same in character and intensity as those listed in Subsection 4.6.2. Such as use is subject to the permit process which governs the category in which it falls.

4.6.5 Site Development Standards

The following matrix contains standards related to development intensity, building setbacks, and building height limitations within the Multi-Family Residential District.

MULTI-FAMILY RESIDENTIAL DISTRICT GENERAL DEVELOPMENT CRITERIA	
Minimum Site Area for New Development	Not Applicable
Lot Coverage	100%
Floor Area Per Dwelling Unit	
A. Two Family Duplex	A. Minimum 800 SF of enclosed net floor area exclusive of garage or carport
B. Each One Bedroom Dwelling Unit	B. Minimum 650 SF of enclosed net floor area exclusive of garage or carport
C. Two Bedroom Dwelling Unit	C. Minimum 750 SF of enclosed net floor area exclusive of garage or carport
D. Bachelor/Studio Dwelling Unit	D. Minimum 450 SF net floor area
E. Each Bedroom in Excess of Two	E. Minimum 50 SF of enclosed net floor area shall be added (excludes stairwell areas of a second floor and any inaccessible space within the D.U.).
F. Single Room Occupancy	F. Minimum 300 SF of enclosed net floor area exclusive of garage or carport
G. Senior Citizen Dwelling Unit	G. Minimum 600 SF of enclosed net floor area exclusive of garage or carport for a two bedroom apartment. Minimum 450 SF for a one bedroom apartment. Minimum 300 SF for a Single Room Occupancy.
Height Limitations: Maximum Stories	36FT/3 Stories
Minimum Landscaping Required within Public ROW (i.e., sidewalk)	One (1) 24-inch Box Tree Per 50 Linear FT of Street Frontage

MULTI-FAMILY RESIDENTIAL DISTRICT GENERAL DEVELOPMENT CRITERIA	
BUILDING SETBACKS	
Front & Streetside Yards (as measured from the street right-of-way line):	
Main Street	10 FT
First Street	20 FT
Third Street	15 FT
Stimson Avenue	15 FT
Albert Street	15 FT
Central Avenue	25 FT
Workman Street	15 FT
Interior Side Yard	5 FT
Rear Yard	15 FT Minimum, unless a reduced yard is necessary to accommodate required on-site parking, circulation aisles, and dedication of alley R.O.W. Rear yard reduction must be processed in accordance with Section 4.1.6 in this Specific Plan

Garage doors facing public rights-of-ways shall be a minimum of twenty (20) feet from the property line the doors are facing and shall be equipped with automatic roll up doors.

4.6.6 Senior Citizen Housing Developments

Any multi-family residential development which is planned specifically as senior citizen housing shall conform to all standards set forth in Chapter 10.58, *Senior Citizen Housing*, of the City's Municipal Code.

4.7 PARKING STANDARDS

4.7.1 Purpose

These regulations are established in order to assure that parking facilities are properly designed and located in order to meet the parking needs created by specific uses, and ensure their usefulness, and protect the public health, safety, and welfare.

4.7.2 Basic Regulations for Off-Street Parking

Off-street parking shall be provided subject to the provisions of this Section for:

- Any new building constructed;
- Any addition or enlargement of an existing building or use except as provided in Section 4.7.4.C; and
- All required parking spaces shall be located on the same building site or development, unless an exception is provided elsewhere in Section 4.7.

All off-street parking spaces and areas required by this ordinance shall be designed and maintained to be fully usable for the duration of the use requiring such areas and spaces.

On-street parking within public or private streets, driveways, or drives shall not be used to satisfy the off-street parking requirements.

Whenever the computation of the number of off-street parking spaces required by this section results in a fractional parking space, one (1) additional parking space shall be required for one-half (1/2) or more fractional parking space and any fractional space less than one-half (1/2) of a parking space shall not be counted.

Parking facilities constructed or substantially reconstructed subsequent to the effective date of this ordinance whether or not required, shall conform to the design standards set forth in this section."

4.7.3 Parking Area Design Standards

Design standards are established by this section to set basic minimum dimensions and guidelines for design, construction, and maintenance of parking within the DBD. The following standards shall apply to all districts within the DBD including the Mixed-Use, Community Service, Office, Business Park and Multi-Family Residential Districts.

AMENDED BY ORD 02-806

A. Standard Parking Spaces:

Each standard parking space shall consist of a rectangular area of not less than nine (9) feet wide by nineteen (19) feet long. All parking spaces should have a vertical clearance of not less than seven and one-half (7 ½) feet.

B. Compact Parking Spaces:

Spaces designated for use by compact cars may be reduced in size to a minimum of eight (8) feet in width and sixteen (16) feet in length. A maximum of 35 percent of all required parking spaces may be composed of compact parking spaces.

C. Handicapped Parking Spaces:

Each parking space designated for use by the handicapped shall consist of a rectangular area not less than fourteen (14) feet wide by nineteen (19) feet long, and shall be located in an area not exceeding two (2) percent slope. All spaces shall be located near or convenient to a level or ramped entrance, not exceeding a five (5) percent slope, to the facility serviced by the parking space. Parking spaces for the handicapped shall be restricted for use by the handicapped only.

D. Aisle Dimensions:

Each parking and loading space shall have adequate drives, aisles, and turning and maneuvering areas for access and usability, in accordance with Table 2, *Overall Parking Bay Width*, on the following page.

E. Paving:

Parking and loading facilities shall be surfaced and maintained with asphalt concrete, or other permanent, impervious surfacing material sufficient to prevent mud, dust, loose material and other nuisances. Alternative surface material can be considered by the Community Development Director, if shown that such material will not cause adverse effects and that it will remain in a usable condition.

F. Parking Bay Width:

Each parking facility is designed with parking bay units. The size or width of this unit is dependent on one or two-way traffic and single or double loaded aisles. Use the following chart to determine the overall width of the parking bay design which is being used. The dimensions listed are the amount necessary to contain parking stall depth and aisle width (see Table 2, below).

**TABLE 2
OVERALL PARKING BAY WIDTH**

PARKING BAY WIDTHS	PARKING ANGLE (IN DEGREES)			
	30	45	60	90
1. Parking bay width for one-way traffic and double loaded aisles:				
a. Compact Parking Space	40 FT	44 FT	49 FT	56 FT
b. Standard Parking Space	43 FT	49 FT	55 FT	62 FT
2. Parking bay width for one-way traffic and single loaded aisles:				
a. Compact Parking Space	26 FT	28 FT	32 FT	39 FT
b. Standard Parking Space	28 FT	30 FT	35 FT	43 FT
3. Parking bay width for two-way traffic and double loaded aisles:				
a. Compact Parking Space	48 FT	52 FT	54 FT	56 FT
b. Standard Parking Space	51 FT	56 FT	59 FT	62 FT
4. Parking bay width for two-way traffic and single loaded aisles:				
a. Compact Parking Spaces	34 FT	36 FT	38 FT	40 FT
b. Standard Parking Spaces	36 FT	38 FT	41 FT	43 FT

G. Striping:

All parking stalls shall be clearly outlined with double lines on the surface of the parking facility or any other permanent space designator (e.g., trees, shrubs, etc.) approved by the Community Development Director. In all parking facilities all aisles, approach lanes, and maneuvering areas shall be clearly marked with directional arrows and lines to expedite traffic movement.

Parking bay widths shall be computed according to the specifications set forth in Table 2, above.

One-way access driveways with no parking and two-way access driveways with no parking and providing access to not more than eight (8) parking spaces shall be a minimum of twelve (12) feet in width. Two-way access driveways with more than eight (8) parking spaces shall be a minimum of twenty (20) feet in width.

H. Parking Lot Striping and Markings:

Parking stall striping, directional arrows and parking stall identification shall meet the following standards:

- All parking stalls shall be painted with a double four (4) inch wide continuous line.

- All aisles, entrances and exits shall be clearly marked with directional arrows painted on the parking surface.
- All compact parking stalls shall be individually labeled with the words "compact car" painted on the parking surface of each stall.
- All handicapped parking stalls shall be individually labeled and signed in accordance with Uniform Building Code and California Vehicle Code standards.

I. Landscaping:

The following basic standards shall be observed:

- One (1) minimum six-foot (6') high tree shall be required per each six (6) parking stalls. Each tree shall be permanently irrigated, double staked and maintained in healthy condition. Trees may be clustered if desired.
- All landscaping shall be protected with concrete curbs.
- Each parking area, not enclosed by either a building or a solid wall (walls shall not exceed height of 42"), shall provide a perimeter landscaped strip at least ten (10) feet wide or hedge of 42" (inside dimension) where the parking area adjoins the street/sidewalk interface. If the parking area is enclosed by a wall or hedge, then the required landscaped strip may be reduced to a minimum of five (5) feet.

J. Planter Design:

All parking lot planters shall be designed to meet the following minimum requirements:

- The Planters shall be separated from maneuvering and parking areas by a six (6) inch raised concrete curb or equivalent.
- Tree planting wells located at the front (head in) of parking stalls shall contain a minimum of twenty-five (25) square feet and the smallest outside dimension shall not be less than five (5) feet.
- Landscape planters along the sides of parking stalls shall contain a minimum of one hundred (100) square feet and the smallest outside dimension (including curbs) shall not be less than six (6) feet in width.

4.7.4 Parking Requirements

The following sections list the required amount of parking for each category of uses, special requirements and optional requirements.

A. Commercial/Office/Light Industrial Uses:

A minimum of one (1) off-street parking stall for each four hundred (400) square feet of gross floor area for all buildings and/or uses. This requirement will apply for all commercial, office and light industrial uses in the DBD; provided, however, that whenever delineation of independent uses is required, the following standards shall apply:

- Barber shops or beauty parlors: one (1) per two hundred (200) square feet.
- Buildings used solely for laundromats or dry cleaning establishments: one (1) for each three hundred (300) square feet.
- Hotels: one (1) parking space for each guest unit and two (2) spaces for resident manager or owner.
- Stores solely for the sale of furniture and appliances: one (1) for each five hundred fifty (550) square feet of gross floor area.

B. Health Uses:

- Dental clinics or offices, medical clinics or offices: one (1) for each three hundred (300) square feet of gross floor area.
- Convalescent and nursing homes, homes of aged, rest homes, children's homes and sanitariums: one (1) for every four (4) beds in accordance with the resident capacity of the home as defined on the required license or permit.
- Health clubs and fitness centers: One (1) for each two hundred fifty (250) square feet of gross floor area.

C. Places of Assembly:

- Restaurants, taverns, lounges and other establishments for the sale and consumption on the premises of food and beverages: one (1) space for every one hundred (100) square feet of gross floor area up to six thousand (6,000) square feet plus one (1) for each additional fifty-five (55) square feet of gross floor area over six thousand (6,000) square feet. No on-site parking shall be required for outdoor dining areas of less than a total of 400 square feet. Any outdoor dining areas above this limit shall be subject to the requirements of this subsection.

908-70 040 BY CEDENW

- Auditoriums: one (1) for each thirty-five (35) square feet of gross floor area.
- Theaters, movies: one (1) space per three (3) seats, plus five (5) for employees for single screen theaters; one (1) space per four (4) seats, plus five (5) for employees for multi-screen theaters.
- Private clubs, lodge halls, union headquarters: one (1) for each seventy-five (75) square feet of gross floor area.

D. *Child Care Facilities:*

- Child care nurseries, including preschools and nursery school: one (1) stall for each staff member, plus one (1) for each five (5) children for which the facility is licensed.

E. *Multi-Family Residential Uses:*

- Bachelor/studio dwelling unit: one (1) parking space per dwelling unit in a garage or carport and one-fourth open parking space per dwelling unit.
- One bedroom dwelling unit: one (1) parking space per dwelling unit in a garage or carport and one-half open parking space per dwelling unit. 1.5
- Two, three or more bedroom dwelling unit: two (2) parking spaces per dwelling unit in a garage or carport and one-half open parking space per dwelling unit. 2.5
- Multi-family residential development(s) containing eight or more dwelling units: in addition to the required parking spaces, one (1) guest parking space shall be provided for each eight (8) dwelling units or fraction thereof.

F. *Senior Citizen Housing Parking Requirements:*

The total amount of required off-street parking spaces for senior citizen housing developments may be reduced up to fifty (50) percent from the amount of parking required under Subsection 4.7.4.E, *Multi-Family Residential Uses*, above, subject to approval by the City Traffic Engineer."

AMENDED BY AD 02-806

4.7.5 Special Requirements

The following parking requirements are applicable to all commercial and office land uses. These special stalls shall be closest to the facility for which they are designed in order to encourage their use.

A. Handicapped Parking:

Those facilities with over 25 or more spaces shall designate two percent of one space, whichever is greater, of the total number of stalls for use by the handicapped. The designation and design shall conform to state standards.

B. Motorcycle Parking:

Motorcycle parking space shall be provided for every 30 automobile parking spaces or portion thereof.

C. Compact Automobile Parking:

Facilities with 20 or more parking spaces may provide up to 35 percent of its parking for use by compact cars.

D. Bicycle Storage:

All commercial and office areas shall provide adequate locking facilities for bicycle parking at any location convenient to the facility for which they are designated. Whenever possible, weatherproofing or facility covering should be used.

4.7.6 Modified Parking Requirements

- A. The amount of required parking for any new individual development project or any building additions constructed after March 10, 2002, or for a group of development projects occurring simultaneously may be modified from the standards set forth in Subsections 4.7.4, *Parking Requirements*, and 4.7.5, *Special Requirements*, above, provided that any petition for such variance shall be accompanied by a parking demand study prepared by an independent traffic engineer licensed by the State of California, or such other study as approve by the City Traffic Engineer and provided to the City by the petitioner at the petitioner's sole expense. The variance shall be granted upon a finding by either the Planning Commission or the City Council that the evidence presented shows that all of the following conditions exist:

- That the variance will not cause an increase in traffic congestion in the immediate vicinity of the project, nor adversely affect any adjoining land uses; and

AMENDED BY CITY 02-806

- That the granting of the variance under the conditions imposed, if any, will not be detrimental to the peace, health, safety or general welfare of the citizens of La Puente.

B. Notwithstanding the requirements of this Chapter and Chapter 10.76 (Nonconforming Buildings and Uses), any building legally constructed prior to March 10, 2002 shall be considered conforming with respect to parking and, if destroyed by fire, earthquake or other act of God, may be reconstructed to its original square footage without conformance to the parking requirements of this Chapter. Any additions to such buildings after March 10, 2002 shall conform to the parking requirements of this Chapter.

AMENDED BY ORD 02-2006

4.7.7 Parking In Lieu Fees

REMOVED
BY
WJW

At the discretion of the Planning Commission, payment of fees may be required in lieu of providing parking spaces in conjunction with new construction, rehabilitation, or land use intensification subject to the provisions of this Section. Criteria used to determine if parking will be reduced and payment of in lieu fees required shall include the following:

- A more appropriate land use pattern emphasizing the storefront image, which will result in maintaining pedestrian circulation.
- Construction of a parking area will be incompatible with adjacent uses.
- Providing required on-site parking results in an inefficient site plan and/or architectural style incompatible with surrounding development.

In lieu of providing each required parking space on-site, a standard fee to be determined by the City Council, shall be substituted subject to approval by the Planning Commission, said fee shall be used in the purchase of land and development of consolidated parking facilities and/or to provide parking enforcement to serve the DBD. In the event the total amount of cash contributions in lieu of on-site required spaces shall be deemed excessive by the City Council, the contributions may be paid in annual installments with interest thereon, upon such terms and conditions to be approved by the City Council. Said agreement shall be recorded and all monies due there under shall be a lien upon the subject property.

4.7.8 Public Off-Street Parking Districts

REMOVED
BY
WJW

Parking spaces provided by a parking assessment district shall be credited to the uses on parcels in the assessment district in proportion to the assessment levied against each parcel or value of land contributed by the owners of such parcel. Such credit shall be rounded to the nearest half-space with not contributing parcel receiving less than one space credit.

Where a plan or program has been adopted for the provision of public parking facilities to serve a commercial or other area, the City Council may, by resolution, determine the probable cost per space of such proposed facilities and establish a fund into which persons required to provide parking under the provisions of this Code may pay such cost as a fee in lieu of providing each required parking space. Such fees shall be used to acquire parking facilities that will reasonably serve the uses for which the spaces are required.

4.8 DISPLAYS ON PUBLIC SIDEWALKS

4.8.1. Purpose and Intent

The purpose of this section is to afford those businesses located in Downtown Business District buildings that have less than two feet of setback from the public right-of-way the opportunity to provide outdoor displays commensurate with, and sensitive to, the historic qualities of the District. These regulations are intended to allow displays utilizing props, stands, or icons with sample merchandise that are arranged in an aesthetically pleasing manner. They do not permit the display of significant quantities of merchandise on standard display racks or shelves or the sale of items on the public sidewalk.

4.8.2. Location and Design Standards

1. Outdoor display areas shall not encroach in front of other businesses, interfere with other merchants' storefront window displays or unreasonably obstruct visibility of other businesses, or obstruct entrances and driveways.
2. Sidewalk displays may not extend more than two feet into the public right-of-way.
3. Outdoor displays shall be limited to a maximum area of one half square foot for every lineal foot of the same store frontage where the display is placed.
4. A clear, unobstructed pedestrian pathway a minimum of five (5) feet wide shall be maintained on the public sidewalk in front of the property.
5. Outdoor displays must not create a vehicle or pedestrian hazard.
6. All outdoor displays shall include baskets, stands, furniture, statuettes, sculptures, or other similar props or devices on or in which merchandise may be displayed or displayed in concert with merchandise to provide visual attraction to the business. (For example, a clothing store sidewalk display consisting solely of a simple rack upon which clothes are hung is unacceptable.)

Props must be constructed of materials such as wicker, wrought iron, stone, full grain hardwood or other traditional materials consistent with the early Twentieth Century period which the Downtown Business District exemplifies. Vending machines, arcade machines or similar devices shall not be permitted under this section.

7. Signs integrated into the display and designed for and oriented to the casual passersby are permitted. Such signs shall not exceed five feet in height or six (6) square feet in area and shall not include text greater than two (2) inches in height.
8. Outdoor displays must be maintained in a clean, neat, and attractive condition, and in good repair at all times.

REPLUMB
BY
WDM

REPLUMB
BY
WDM

4.8.3 **Permit Required**

Prior to placement of any outdoor displays in the public right-of-way, an encroachment permit, for which there may be a permit issuance fee established by City Council resolution, must be approved by the Community Development Department. Approval of a promotional advertising permit shall be contingent upon the provisions of this subsection and the following requirements:

1. A permit for an outdoor display may be approved for a maximum of one (1) year. Permit renewals may be granted for one (1) year periods. The City reserves the right to temporarily suspend the permit upon reasonable notice because of anticipated or actual conflicts in the use of sidewalk areas due to street repairs, parades, festivals, emergencies, and other similar events. The permit may be revoked for violation of applicable terms and codes.
2. In addition to a permit application form provided by the Community Development Department, the applicant shall submit a detailed plot plan, drawn to scale, illustrating the full width of the sidewalk next to the building in which the applicant business is located and demarcating the area to be used for outdoor display prior to set-up. The Community Development Department may approve the permit subject to reasonable conditions of approval to protect the public health, safety and welfare and assure outside display areas comply with all applicable City Codes.
3. The applicant must comply with applicable insurance requirements as established by the City's Risk Manager."

ADDED BY CHD 01-2006

5.0 DESIGN GUIDELINES

This section of the Las Puente Downtown Business District Specific Plan contains the design guidelines for all new construction and redevelopment within the DBD. The provisions herein will implement the City of La Puente's General Plan and are tailored to accomplish the goals, objectives and policies specified in Section 3. The intent is to establish a distinctive, well designed and high quality environment which enhances the existing character and small town feel of the DBD.

5.1 GENERAL SITE PLANNING GUIDELINES

Design standards are established in this section to serve as general site planning guidelines for development of the mixed use, office and light industrial, and multi-family land uses. The following standards shall apply to all development within the DBD.

5.1.1 Mixed-Use Development

A. *Integrated Structures:*

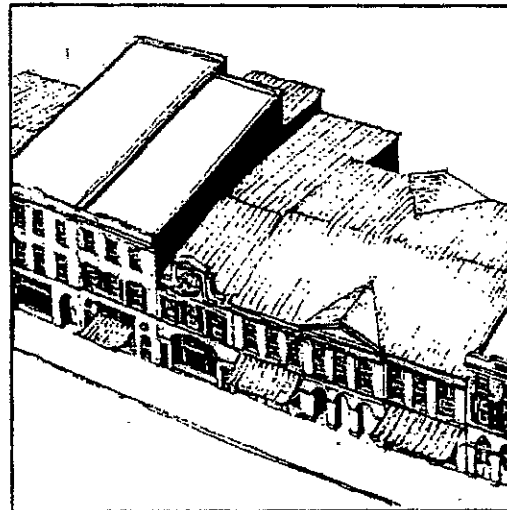
Multi-family residential uses such as stacked flats and apartments may be integrated into the same structures as office and commercial uses.

B. *Mix of Uses:*

Retail uses shall be concentrated on the first floor of all buildings fronting on Main Street. Office and/or residential uses may be constructed over the retail uses.

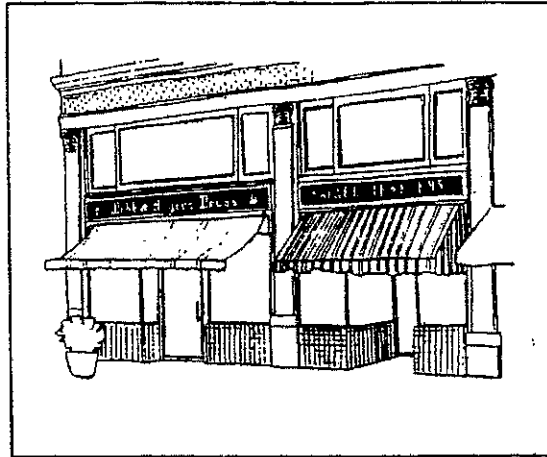
C. *Separate Residential Access:*

All residential units shall have separate access from adjoining commercial and office uses, although internal access between the various uses need not be precluded. Residential access shall be clearly discernable to be different from commercial and office access points.



D. Ground Floor Building Treatment:

Buildings with long expanses of blank walls at the ground floor level along Main Street should be avoided. Instead, the ground floor of mixed-use buildings should be designed with large windows and variable facades. Awnings and canopies are encouraged to be integrated into structures to provide visual interest and continuity between different buildings, especially on Buildings fronting on Main Street.



E. Cornices, Pediments and Parapets:

Buildings fronting on Main Street, in particular, should incorporate cornices, pediments and parapets to screen rooftop equipment and create visual interest. These treatments can also be to serve as visual bridges between one and two story buildings.



F. Street Furniture:

Lighting fixtures, benches, bus shelters, waste containers and signage should be designed to reinforce the project theme, while creating a pedestrian-friendly environment throughout the DBD.

5.1.2 Office and Light Industrial Development

A. Service Areas:

Service areas and special equipment shall be screened from view with landscaping and architectural features.

B. Loading Areas:

Loading areas are to be entirely on-site. No off-site loading areas will be permitted.

C. Refuse Collection:

Refuse collection areas shall be screened whenever possible with a solid perimeter wall using materials and colors compatible with those of adjacent buildings.

5.1.3 Multi-Family Residential Development

A. Building Heights:

Provide a mix of two and three story buildings to minimize long, unbroken facades.

B. Building Orientation:

Orient building toward street with parking in rear of building.

C. Unit Identification:

Provide each unit with its own entry and identity.

5.1.4 Defensible Space

In order to maximize safety and facilitate security surveillance by police personnel, building entries for housing, commercial and office developments should be designed to be easily visible from surrounding streets, accessways and public areas.

5.2 ARCHITECTURAL GUIDELINES

This subsection of the Design Guidelines contains general architectural guidelines that apply to the Specific Plan area. These Design Guidelines discuss appropriate elements for architecture, including building massing, colors, materials, and rooflines. These guidelines do not supercede City, County or State building codes.

New buildings should be designed to blend into the existing architectural context of adjacent and nearby buildings. For example, a new two-story building located adjacent to a one story building could have a one story wing constructed next to the existing building. In this way, the one story wing would serve as a transition from the existing one-story building to the two-story portions of the new building.

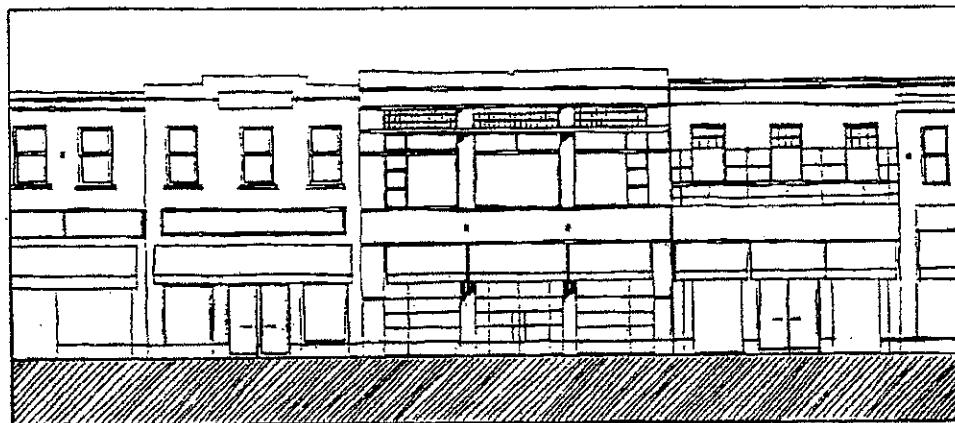
In addition, using similar or complementary roof styles and building materials or colors can help new structures blend into the existing urban fabric of the DBD. New buildings need not copy or duplicate specific design elements or features of existing structures within the DBD. However, designing buildings with similar or complementary building massing, window/door spacing, and materials/colors will help maintain the overall character and charm of the downtown area. Multi-family residential structures, in particular, should be designed to reflect the materials and colors of adjacent and nearby residential buildings, whenever possible.

5.2.1 Residential and Non-Residential Structures

The following guidelines apply to all structures within the DBD. These guidelines are not intended as requirements, although buildings should seek to incorporate some or all of these objectives to mitigate the impact of building massing and to create variety, interest and community compatibility.

A. Reduce Building Scale:

Break-up large building masses into several smaller ones to visually diminish the scale of a building. Building masses can be varied in form or divided to express various interior building functions.



B. Architectural Design Consistency:

All sides of each building shall be consistent in design.

C. *Architectural Projections:*

Facades shall be interrupted with both horizontal and vertical design elements to eliminate flat appearances. Building off-sets, balconies, patios, decorative entrances, recessed doorways/windows, and other design features shall be used to create wall articulation and visually interesting designs.

D. *Architectural Details:*

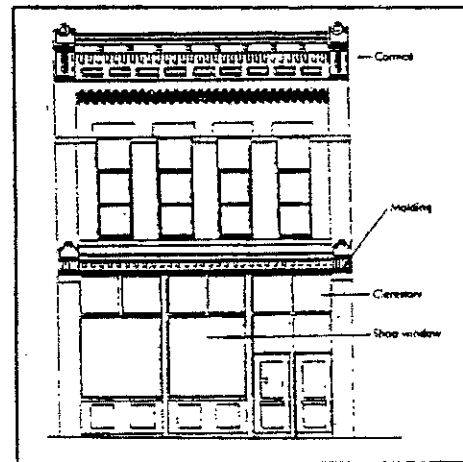
All buildings shall incorporate architectural details to create a sense of interest and to provide for increased sense of pedestrian scale.

E. *Building Height:*

Buildings shall be of one and two stories in height. Certain multiple-family residential areas may contain buildings with a maximum height of three stories.

F. *Roofs and Roofing Materials:*

Flat roofs are only permitted if completely surrounded and screened by a mansard roof or parapet wall. Metal roofing, asphalt shingle, high contrast roofing, colored glazed roofing, highly reflective materials and illuminated roofing are discouraged.



G. *Roof Drains:*

Roof drains shall be designed as an integral part of the structure.

H. *Mechanical Equipment:*

Mechanical equipment shall not be permitted within a required setback, and must be screened from view. Roof mounted equipment is only permitted if it is screened from view by the architectural design of the building. Structures that are clearly recognizable as screening rather than an integral part of the architecture of the building by virtue of its design and materials shall not be permitted.

I. Paving:

Concrete, colored brick or pavers (no asphalt paving). Large concrete areas shall be visually broken up with colored brick or pavers.

J. Orientation:

Elevations facing public streets shall be treated as front elevations.

*K. Patios, Verandas,
Courtyards and
Trellises:*

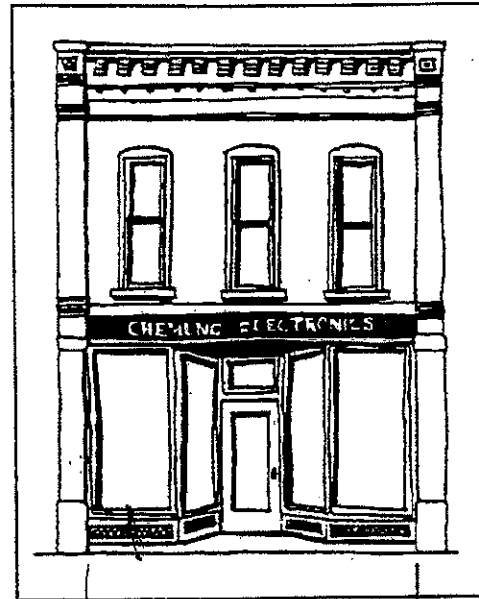
Patios, verandas, courtyards and trellises are encouraged to create attractive pedestrian scale spaces.

L. Entrances:

Accented with decorative doorways and doors, i.e. carved panels are encouraged.

M. Colors:

White, light earth tones, natural wood colors are encouraged.



N. Rooftop Signage:

No building signage shall be located and/or mounted on any roof within the DBD.

5.2.2. Additional Non-Residential Design Guidelines

A. Architectural Style:

Early 20th Century Main Street

B. Materials:

Materials shall be consistent with the architectural style. Brick, stone (or simulated stone), slumpstone, board and batten and textured stucco or combinations thereof as appropriate for the design of the building are encouraged.

C. *Roofing:*

Flat roofs with parapets are encouraged in non-residential buildings. Sloping roofs over entries, walkways are encouraged to provide entry identification and comfortable, shaded, walking paths.

D. *Windows:*

Windows shall be designed with clear or lightly tinted glazing. Heavily tinted windows and reflective windows are discouraged. Large storefront show windows on the ground floor of buildings are encouraged in retail and mixed-use buildings to stimulate pedestrian interest.

E. *Awnings:*

Fabric awnings are encouraged. Metal awnings are prohibited.

5.2.3. Additional Residential Design Guidelines

A. *Architectural styles:*

Spanish or Mediterranean architectural styles are encouraged.

B. *Materials:*

Materials shall be consistent with the architectural style (i.e., Spanish and Mediterranean - stucco walls, tile roofing, wood paneled doors, etc.)

C. *Textures:*

Heavy stucco, random or irregular patterns

D. *Roofing:*

Clay "S" tile roofing is encouraged for Spanish or Mediterranean style buildings. Flat roofs are discouraged.

E. *Windows:*

Windows shall be designed with clear or lightly tinted glazing. Heavily tinted windows and reflective windows are discouraged. Windows shall be in proportion to the walls in which they are located. Small windows or windows with window mullions that divide larger glazed panes are encouraged. French or sectional doors are encouraged. Large sliding glass doors are discouraged.

F. *Balconies:*

Balcony and second story window details may incorporate wrought iron details if compatible with the building architecture. Railings may also be

constructed of painted or stained wood or painted ornamental steel. Balconies are encouraged on multiple-family projects to provide residents on the upper stories with private outdoor space.

G. Awnings:

Fabric awnings are encouraged. Metal awnings are prohibited.

H. Patio structures:

Patio structures, covers, trellises and similar structures shall be designed and constructed to complement the architectural design of the main structure. Such structures shall be made of wood, masonry (such as slump stone, brick, etc.), stucco, and may incorporate canvas as a roofing element. Metal type patio covers are not permitted.

I. Design features:

Projects shall include such design elements that are consistent with the Spanish or Mediterranean architectural theme. Such elements may include, but are not limited to, iron work, decorative outdoor lighting fixtures, decorative walkway paving and patios, water features, etc.

J. Landscaping:

Landscaping should be an integral part of each project and shall compliment the design of the buildings. In higher density residential developments, open space is at a premium and it is all the more important to make the most out of the limited areas that are available. Professionally prepared concept plans should be reviewed and approved as part of the design review process.

5.2.4 Examples of Architectural Design Elements

To aid prospective project applicants in designing projects that will meet the intent of the Architectural Guidelines of the DBD, examples of Spanish and Mediterranean architecture illustrative of design elements that should be incorporated into projects are included in Appendix "A" of this Specific Plan.

5.3 PARKING GUIDELINES

It is the intent of this Specific Plan to encourage innovative parking solutions throughout the Specific Plan area. The provision of vast amounts of parking consumes valuable land, results in major development costs, and promotes continued reliance on the automobile. Where feasible, the following parking solutions are encouraged:

5.3.1 Parking Criteria

A. *Shared Parking:*

Because certain active uses function differently and attract people at different times of the day and night, the possibility of two or more uses sharing the same parking area should be examined and implemented, subject to the approval of the City Engineer. For example, offices and daytime-oriented retail uses could share parking facilities with night clubs or theaters. Once uses are established, changes in types of uses sharing the same parking area shall require preparation of a shared parking study, unless the new types of uses are essentially similar to the original types of uses.

B. *Joint-Development Parking Districts:*

Owners of two or more contiguous properties may form a parking district and develop a comprehensive parking plan that will allow them to coordinate their parking requirements, not necessarily tied to individual zoning requirements. Creation of a parking district and any parking plan will require the approval of the Community Development Director.

C. *Reciprocal Access:*

Owners of adjacent properties are encouraged to explore the feasibility of providing reciprocal access between parcels. The intent of the reciprocal access is to promote the concept of shared vehicular and pedestrian ingress/egress across property lines. Reciprocal access agreements would also provide opportunities for more effective utilization of the properties and reduce the land area necessary to accommodate driveways.

Applicants for new construction or alteration of existing structures over and above 50 percent of the size of the original structure shall attempt to provide reciprocal access with adjacent properties, prior to filing a development plan application with the City. Whenever adjacent property owners have expressed an interest and willingness to participate in the establishment of reciprocal access, project plans shall be designed to include such reciprocal access, and the recording of such agreement. If an agreement cannot be reached regarding the establishment of reciprocal access, the project may be approved without such agreement being provided.

D. *Public Off-Street Parking Districts:*

Parking spaces provided by a parking assessment district shall be credited to the uses on parcels in the assessment district in proportion to the assessment levied against each parcel or value of land contributed by the owners of such parcel. Such credit shall be rounded to the nearest half-space with not contributing parcel receiving less than one (1) space credit.

Where a plan or program has been adopted for the provision of public parking facilities to serve a commercial or other area, the City Council may, by resolution, determine the probable cost per space of such proposed facilities and establish a fund into which persons required to provide parking under the provisions of this Code may pay such cost as a fee in lieu of providing each required parking space. Such fees shall be used to acquire parking facilities that will reasonably serve the uses for which the spaces are required.

E. Parking In Lieu Fees:

At the discretion of the Planning Commission, payment of fees may be required in lieu of providing parking spaces in conjunction with new construction, rehabilitation, or land use intensification subject to the provisions of this Section. Criteria used to determine if parking will be reduced and payment of in lieu fees required shall include the following:

- A more appropriate land use pattern emphasizing the storefront image, which will result in maintaining pedestrian circulation.
- Construction of a parking area will be incompatible with adjacent uses.
- Providing required on-site parking results in an inefficient site plan and/or architectural style incompatible with surrounding development.

In lieu of providing each required parking space on-site, a standard fee to be determined by the City Council, shall be substituted subject to approval by the Planning Commission. Such fee shall be used for the purchase of land and development of consolidated parking facilities and/or to provide parking enforcement to serve the DBD. In the event the total amount of cash contributions in lieu of on-site required spaces shall be deemed excessive by the City Council, the contributions may be paid in annual installments with interest thereon, upon such terms and conditions to be approved by the City Council. Said agreement shall be recorded and all monies due thereunder shall be a lien upon the subject property.

5.3.2 Off-Street Parking and Loading Design

A. Paving Surfaces:

Parking and loading facilities shall be surfaced and maintained with asphaltic, concrete or other permanent, impervious surfacing material sufficient to prevent mud, dust, loose material and other nuisances. Alternative surface material can be considered by the Community Development Director, if shown that such material will not cause adverse effects and that it will remain in a usable condition.

B. Parking Area Lighting:

Please see Section 5.6, *Lighting Guidelines*.

C. Parking Area Noise:

Areas used for primary circulation for frequent idling of vehicle engines, or for loading activities shall be designed and located to minimize impacts on adjoining residential properties, including provisions for screening and sound baffling.

D. Parking Area and Loading Area Drainage:

All parking and loading facilities shall be provided with permanent storm drainage facilities. Surfacing, curbing, and drainage improvements shall be sufficient to convey the free flow of water away from the site to public storm drain facilities to preclude standing pools of water within the parking facility.

E. Safety Features:

Parking and loading facilities shall meet the following standards:

- Safety barriers, protective curbing, and directional markers shall be provided to assure pedestrian/vehicular safety, efficient utilization, protection to landscaping, and to prevent encroachment onto adjoining public or private property.
- Visibility of pedestrians, bicyclists and motorists shall be assured when entering individual parking spaces, when circulating within a parking facility, and when entering an exiting a parking facility.
- Internal circulation patterns, and the location and traffic direction of all access drives, shall be designed and maintained in accord with accepted principles of traffic engineering and traffic safety.

F. Screening of Parking Areas:

Unenclosed off-street parking areas shall be screened from view from public streets and adjacent more restrictive land uses. Screening shall consist of the following materials:

- Planting materials, when used as a screen, shall consist of compact evergreen hedges in association with parking lot trees. The hedges shall be planted in such a manner, so as to provide screening and reach a minimum height of two (2) feet within eighteen (18) months.

- Parking areas located adjacent to the street/sidewalk – low profile walls not to exceed three and one-half (3 ½) feet in height consisting of masonry material.
- Parking areas located or constructed adjacent to existing residential neighborhoods shall be screened from view by either a minimum six (6) foot high solid block wall, a landscaped berm of at least twenty (20) feet in width and three (3) feet in height (required width and height refers to berm only, exclusive of landscaping), or a combination of a wall and berm that effectively forms a minimum six (6) foot high barrier. Walls shall be either covered with stucco, or constructed with decorative block or a similar material.

G. Striping:

All parking stalls shall be clearly outlined with double lines on the surface of the parking facility or any other permanent space designator (trees, shrubs, etc.) approved by the Community Development Director. In all parking facilities all aisles, approach lanes, and maneuvering areas shall be clearly marked with directional arrows and lines to expedite traffic movement.

H. Maneuvering:

Parking and maneuvering areas shall be arranged that any vehicle entering a public right-of-way on a major or secondary street can do so traveling in a forward direction.

I. Planter Design:

All parking lot planters shall be designed to meet the following minimum requirements:

- Planters shall be separated from maneuvering and parking areas by a six (6) inch raised concrete curb or equivalent.
- Tree planting wells located at the front (head in) of parking stalls shall contain a minimum of twenty-five (25) square feet and the smallest outside dimension shall not be less than five (5) feet.
- Landscape planters along the sides of parking stalls shall contain a minimum of one hundred (100) square feet and the smallest outside dimension (including curbs) shall not be less than six (6) feet in width.
- Pedestrian walkways shall be provided in landscaped planters along the sides of parking stalls.

J. Parking Lot Striping and Markings:

Parking stall striping, directional arrows and parking stall identification shall meet the following standards:

- All parking stalls shall be painted with a double four (4) inch wide continuous line.
- All aisles, entrances and exits shall be clearly marked with directional arrows painted on the parking surface.
- All compact parking stalls shall be individually labeled with the words "compact car" painted on the parking surface of each stall.
- All handicapped parking stalls shall be individually labeled and signed in accordance with Uniform Building Code and California Vehicle Code standards.

5.4 LANDSCAPE DESIGN GUIDELINES

Landscaping within the DBD will help create an atmosphere conducive to pedestrian activities, define specific areas, provide transitions between neighboring properties, and buffer or screen loading and storage areas from public view. The La Puente DBD Specific Plan includes a schematic plan as shown in Figure 11, *Schematic Plan*. This plan provides an overall landscape concept for the DBD and identifies proposed enhanced streetscape areas, focal points and special intersection treatments, a gateway feature and pedestrian walkways throughout the downtown area. The landscape development concept shall provide a continuous and harmonious setting for the DBD, enhancing the overall community character. The schematic plan has been designed to accomplish the following goals:

- 1) To strengthen and enhance the identity of a small town pedestrian movement;
- 2) To identify special features and landscape treatments such as the gateway entrance feature and the central public plaza;
- 3) To compliment the architectural theme of the DBD by selection of street trees, hardscape materials and street furniture;
- 4) To enhance the motorist's sense of arrival into the downtown; and
- 5) To enhance the unique sense of place and identity of the downtown.

5.4.1 Streetscape Design Concepts

The information provided in this section identifies the streetscape design elements of paving (sidewalks and crosswalks), street trees, street

furniture, lighting, special parking, and entry "gateway" treatments. The streetscape design concepts shall apply to all applicable areas of the public right-of-way within the DBD.

A. *Architectural Theme:*

Selected street furniture shall be compatible with the overall architectural theme, and shall be of a simple style.

B. *Color:*

Street furniture shall be of a dark or earthtone color, compatible with the color palette of adjacent buildings.

C. *Seating Areas:*

Seating areas should be provided along the sidewalk at key intersections.

D. *Pavement Treatment:*

Accent paving, including textures and colored concrete and interlocking pavers, shall be used at special intersection treatment areas.

5.4.2 General Landscape Design Guidelines

A. *Landscape Form:*

Landscaping may be provided within the DBD in the form of street trees, planter beds, raised planters, containers, or window boxes.

B. *Landscape Hierarchy:*

Landscaped areas should generally consist of grasses and groundcovers, shrubs and trees.

C. *Design Concepts:*

The following landscape design concepts and recommendations are encouraged within the DBD:

- Use specimen trees at special intersection treatment areas and in the public plaza.
- Incorporate landscaping to identify entrances, separate outdoor dining areas, shade courtyards or define alleys to encourage pedestrian activity.
- Screen trash, equipment, storage and loading areas from public view by using plant materials.

- Use flowering plant material throughout the DBD, where appropriate.
- Install vines on both walls and verandas.
- Plant canopy and shade trees in parking areas and in the public plaza.
- Utilize street trees with a see-through quality.
- Preserve and protect existing trees and shrubs as possible, since established plants are often adapted to low water using conditions.
- Incorporate pervious paving materials wherever feasible to reduce surface water runoff and to aid in ground water recharge.

D. Water Conservation Techniques:

All landscaped areas should emphasize the following water conservation techniques:

- Limit the number and size of turf areas.
- Install efficient irrigation systems.
- Use mulch to protect and cool the soil.
- Use drought tolerant and hardy plants.
- Establish a regulated maintenance schedule.

E. Irrigation Systems:

The landscape irrigation system shall be automatic and designed to minimize over spray run-off. All irrigation systems shall be designed to minimize vandalism by placing controls in appropriate above or below grade enclosures. Efficient irrigation systems will help to minimize runoff and evaporation, while maximizing the amount of water that reaches the plant roots. Drip irrigation, soil moisture sensors and automatic irrigation systems are a few methods of increasing irrigation efficiency.

F. Reclaimed Water:

The feasibility of using reclaimed wastewater, stored rainwater or grey water for irrigation for DBD projects should be investigated by the City in conjunction with the La Puente Valley Water Company. Sources of irrigation water should be used if readily available.

5.4.3 Recommended Street Tree Plant Palette

STREET TREES

STREET/LOCATION	BOTANICAL NAME	COMMON NAME
Main Street	<i>Magnolia grandiflora</i>	Southern Magnolia
Workman Street	<i>Liquidambar styraciflua</i>	American Sweet Gum
Stimson Avenue	<i>Liquidambar styraciflua</i>	American Sweet Gum
Albert Street	<i>Liquidambar styraciflua</i>	American Sweet Gum
Old Valley Boulevard	<i>Cupaniopsis anacardiodes</i>	Carrotwood
Glendora Avenue	<i>Cupaniopsis anacardiodes</i>	Carrotwood
Rowland Street	<i>Cupaniopsis anacardiodes</i>	Carrotwood
Central Avenue	<i>Tristania conferta</i>	Brisbane Box
First Street	<i>Koelreuteria bipinnata</i>	Chinese Flame Tree
Second Street	<i>Koelreuteria bipinnata</i>	Chinese Flame Tree
Third Street	<i>Koelreuteria bipinnata</i>	Chinese Flame Tree
Fifth Street	<i>Koelreuteria bipinnata</i>	Chinese Flame Tree
Civic Center Complex/ Public Park	<i>Pyrus calleryana</i> 'Bradford' <i>Pinus canariensis</i> <i>Tristania conferta</i>	Ornamental Pear Canary Island Pine Brisbane Box

SPECIMEN TREES

Botanical Name	Common Name
<i>Albizia julibrissin</i>	Silk Tree
<i>Arecastrum romanzoffianum</i>	Queen Palm
<i>Cinnamomum camphora</i>	Camphor Tree
<i>Jacaranda mimosifolia</i>	Jacaranda
<i>Lagerstroemia indica</i>	Crape Myrtle
<i>Liriodendron tulipifera</i>	Tulip Tree

5.4.3 Street Furniture

Street furniture such as bollards, detailed lighting fixtures, benches, bus shelters, waste containers, telephone booths/stands, bicycle racks, pottery and tree grates shall be designed to reinforce the downtown theme.

5.5 BUILDING SIGNAGE

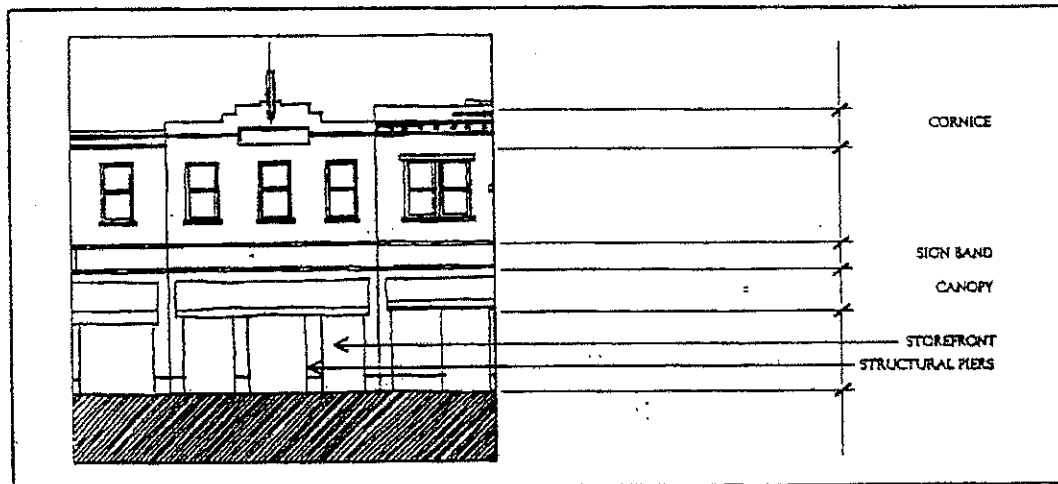
Building signage within the DBD is intended to provide business identification as its primary purpose rather than signage to convey a message or advertisement of a product. This section also recognizes and strongly subscribes the right to identify a business, but to keep such rights within reasonable guidelines to ensure the DBD sign regulations are consistent with the overall design theme of the DBD.

5.5.1 Signage Criteria: Non-Residential

Building signage shall be permitted on retail, office community service, light industrial and public buildings and/or land uses as allowed for in Section 3.0 – *Specific Land Use Plan*. Identification signs will be divided into two categories; motorist orientation and pedestrian orientation. Each category shall compliment the architectural theme or style of the building and adjoining streetscape.

A. *Building Identification Signs – Motorist Orientation:*

- A single sign band shall be created for sign purposes and shall be a minimum of 8'-0" above the finish sidewalk grade adjacent to the building frontage.



- A variety of types of signage may be incorporated into this sign band, provided that the sign band is visually integrated into the building as whole. A uniform sign program shall be encouraged whenever possible.

- The maximum sign area shall be 1.5 square feet per building frontage or business frontage, whichever is less. Corner buildings are permitted a second sign of equal or lesser size than the sign on the building frontage.

B. Building Identification Signs – Pedestrian Orientation:

- No more than one (1) business identification sign shall be permitted on a window or door of a business. Said sign shall be limited to a maximum of three (3) square feet per sign.
- Advertisement and/or business identification signs on awnings are permitted so long as the letters do not exceed six (6) inches in height.
- Business identification signs that project from the building (perpendicular to the building front) may be permitted. Said sign shall not be greater than two (2) square feet in sign area per building and shall not be less than 8'-0" from the finished sidewalk in front of the building. Signs of this type shall be made of carved wood, brass or other decorative material. Business identification signs that project from the building may be a symbol or product sold in lieu of the written word.
- Buildings located on a corner lot may be permitted to have pedestrian-oriented signs on the secondary frontage equal to the criteria for the building frontage.

C. Freestanding Identification Signs:

- Only one (1) freestanding identification sign shall be permitted per property on which a business and/or building is located. Such sign shall only relate to the business upon the lot it is intended to serve.
- Each freestanding identification sign shall have not more than one-half (1/2) square foot of sign area for each linear foot of lot frontage per side of a double-faced sign. However, no sign face shall be greater than twenty-five (25) square feet.
- Each freestanding identification sign shall be located in a landscape planter equal to twice the total sign face area of a double-faced sign. The landscape planter shall be of live material and will be in addition to any other landscaping required. All landscaping shall be automatically irrigated.
- The maximum height of the freestanding identification sign shall be six (6) feet above the finish grade of a sidewalk or ground level.

- The address of the building shall be attached to the freestanding sign. Said numerals shall be a minimum height of four (4) inches.

5.5.2 Signage Criteria: Residential

Building signage shall be permitted for residential buildings and/or land use as allowed for in Section 3.0 - *Specific Land Use Plan*. Identification signs shall be incorporated into the design of the building/land use, complimenting the design of the building.

A. *Building Identification Signs:*

- Each building identification sign shall have not more than one (1) square foot of sign area for each linear foot of building frontage.
- Buildings located on a corner lot may be permitted an additional building identification sign on the side of the building that fronts on the side or cross street. The area of such sign shall be not greater than the sign area permitted on the building frontage.

B. *Freestanding Identification Signs:*

- Only one (1) freestanding identification sign shall be permitted per property on which a business and/or building is located. If the property is improved with a freestanding sign for a non-residential building and/or land use, no additional freestanding sign shall be permitted.
- Each freestanding sign shall have not more than one-half (1/2) square foot of sign area for each linear foot of lot frontage per side of a double faced sign. However, no sign face shall be greater than twenty (20) square feet in area.
- Each freestanding sign shall be located in a landscape planter equal to twice the total sign face area. The landscape planter shall be of live material and shall be in addition to any other required landscaping. All landscaping shall be automatically irrigated.
- The maximum height of the freestanding sign shall be six (6) feet above the finish grade of a sidewalk or ground level.
- The address of the building shall be attached to the freestanding sign. Said numerals shall be a minimum height of four (4) inches.

5.5.3 Promotional Advertising Devices

Promotional advertising devices for grand openings, seasonal sales, and/or special events for a business in the DBD shall be permitted as set forth in the La Puente Municipal Code, except for temporary window advertisement. Such temporary window advertisement shall be prohibited.

5.5.4 General Sign Regulations

Sign regulations that are not provided for by this Specific Plan shall be in accordance with all applicable sign regulations set forth in Chapter 10.74 of the La Puente Municipal Code.

5.6 LIGHTING GUIDELINES

Lights provided to illuminate any parking facility or paved area shall be designed to reflect away from residential uses and motorists. It is the intent to maintain light standards in a low profile design and to be compatible to the architectural design. Light standards shall not exceed 15 feet in overall height from the finished grade of the parking facility. No lighting shall create illumination on adjacent properties which exceeds five foot candles. Lights may be incorporated into bollards for pedestrian lighting, and can be wall mounted to illuminate outdoor steps.

5.7 DEVELOPMENT/DESIGN REVIEW

This section establishes the review procedures for development and sign proposals within the DBD to facilitate project review by local responsible agencies and the City in a timely and efficient manner; to ensure that development projects comply with all applicable local design guidelines, standards, and ordinances; to minimize adverse effects on surrounding properties and the environment; and are consistent with the General Plan which promotes high aesthetic and functional standards to complement and add to the physical, economic, and social character of La Puente.

In addition, the City finds that a Development/Design Review process will support the implementation of the Specific Plan and General Plan, as it stresses quality community design standards. The City further finds that the quality of certain commercial and office uses has a substantial impact upon the visual appeal, environmental soundness, economic stability, and property values of the City. This Section is not intended to restrict imagination, innovation or variety, but rather to focus on DBD design principles which can result in creative imaginative solutions for the project and quality design for the City. It is, therefore, the purpose of this section to:

- 1) Recognize the interdependence of land values and aesthetics and provide a method by which the City may implement this interdependence to its benefit.

- 2) Encourage the orderly and harmonious appearance of structures and property within the DBD along with associated facilities, such as signs, landscaping, parking areas, and streetscapes.
- 3) Maintain the public health, safety and general welfare, and property throughout the DBD.
- 4) Assist private and public developments to be more cognizant of public concerns for the aesthetics of development.
- 5) Reasonably ensure that new developments, do not have an adverse aesthetic, health, safety or architecturally related impact upon existing adjoining properties, or in the City in general.
- 6) Implement those sections of the City's General Plan which specifically refer to the preservation and enhancement of the particular character and unique assets of the DBD and its harmonious development.

5.7.1 Project Requiring Development/Design Review

An application for Development/Design Review is required for all projects involving the issuance of a building permit for construction or reconstruction of a structure, the installation of awnings and signs, new landscaping, parking lots, the conversion of a building from one land use to another, facade remodeling, or alteration of a historical building.

5.7.2 Authority

A. *Development Review Board:*

Development/Design Review applications which meet any of the following criteria shall require review and consideration by the City's Development Review Board (DRB):

- Facade improvements which include slight alterations to an existing building that requires a building permit to be issued.
- The demolition of an existing structure where a new building is constructed in its place.
- All additions that exceed fifty (50) percent of the existing structure's floor area.
- Any project requiring preparation of an EIR shall require Planning Commission review.
- All residential projects and projects with a residential component.

- Facade improvements that would project onto the public right-of-way.

The DRB is authorized to approve or deny applications and to impose reasonable conditions upon such approval, subject to the right of appeal. Conditions may include, but shall not be limited to requirements for architectural compatibility and compliance with design guidelines and criteria, open space, screening and buffering of adjacent properties and walls; requirements for installation and maintenance of landscaping requirements for street improvements and dedications, regulation of vehicular ingress and egress, and traffic circulation; regulation of signs; requirements for maintenance of landscaping and other improvements; establishment of development schedules or time limits for performance of completion; and such other conditions as the DRB may deem necessary to ensure compatibility with surrounding uses and to preserve the public health, safety and welfare.

B. Development Review Committee:

Development/Design Review applications which do not require DRB review as described above shall be subject to review and approval by the Development Review Committee (DRC). Although, if in the opinion of the DRC the application involves unusual site development requirements or unique operating characteristics, or raises questions of development policy substantially more significant than generally pertain to applications for development review and which require DRB consideration, the Community Development Director shall refer the application to the DRB for review and approval.

Development/Design Review applications which meet any of the following criteria shall require review and consideration by the Development Review Committee:

- The installation of signs.
- Modification and/or changes in the location of landscape planters or the installation of new landscape planters or irrigation.
- The installation of awnings.
- The changing of windows and doors and/or the associated trim, treatment, or architectural style.
- Architectural improvements to the facade of a building that do not require the issuance of a building permit.

The Community Development Director may, contingent upon environmental clearance by the Planning Commission, grant approval, or grant approval in a modified form or subject to conditions, or may deny the application. Conditions may include, but shall not be limited to architectural compatibility and compliance with design guidelines and criteria; requirements for open space; screening and buffering of adjacent properties and walls; requirements for installation and maintenance of landscaping and erosion control measures; requirements for street improvements and dedications, regulation of vehicular ingress, egress, and traffic circulation; regulation of signs; regulation of hours or other characteristics of operation; requirements for maintenance of landscaping and other improvements; establishment of development schedules or time limits for performance or completion; and such other conditions as the Community Development Director may deem necessary to insure compatibility with surrounding uses and to preserve the public health, safety and welfare.

5.7.3 Application for Development/Design Review

An application for Development/Design Review shall be filed with the Planning Division in a manner prescribed by the Community Development Department.

5.7.4 Development/Design Review Procedure

A. Scheduling for Development Review Board:

Upon acceptance of a complete application for Development Review Board (DRB) consideration, a project shall be set on the first available agenda for DRB review. The applicant and any persons requesting notice will be notified at least ten (10) days prior to the meetings. In addition, the property shall be posted at least ten (10) days prior to the meeting.

Review and analysis by the DRB will consider design elements, such as, but not limited to, compatibility of the project to surrounding properties; relationship of the project to the design and development criteria provided in the Specific Plan; relationship of the design and layout of the project to the site; architectural design and use of materials; landscaping; screening and buffering techniques of adjacent properties; signs; and open space. The DRB will determine if the project adequately meets City design guidelines and standards.

The recommendation of the DRB will be based upon the project conforming to the following criteria:

- The design and layout of the proposed development is consistent with the applicable elements of the City's General Plan, the development standards and design guidelines as set forth in the La Puente Downtown Business District Specific Plan, and any adopted architectural criteria for specialized area, such as designated historic structures and theme areas.

- The design and layout of the proposed development will not unreasonably interfere with the use and enjoyment of neighboring existing or future developments, and will not create traffic or pedestrian hazards.
- The architectural design of the proposed development is compatible with the character of the surrounding neighborhood and will maintain the harmonious, orderly and attractive development contemplated by this Specific Plan and the City's General Plan.
- The design of the proposed development would provide a desirable environment for its occupants and visiting public as well as its neighbors through good aesthetic use of materials, texture and color that will remain aesthetically appealing and will retain a reasonably adequate level of maintenance.

If the project is found to be not acceptable by the DRB, the Community Development Director will inform the applicant of the design issues and may suggest possible alternatives which would improve the design of the project. The applicant may appeal the DRB's decision to the City Council if a written appeal is submitted to the Community Development Director within ten (10) days of the action by the DRB.

B. Development Review Committee:

The DRC is comprised of members of the Community Development Department, Building and Safety Division and the Sheriff's Department. Review by this Committee will consider items, such as, but not limited to circulation; street improvements; right-of-way dedication; utility easements; grading; drainage facilities; storm drain improvements; uniform building code requirements; security; fire flee; emergency access; location of fire hydrants; water and sewer line connections and sizing; water pressure; permit fees; streetscape and landscape standards; setbacks; parking; and requirements for environmental processing. The Committee will require changes in any development for compliance with adopted codes and standards. They may also make recommendations to the DRB on any policy issues or areas not covered by existing codes and standards.

Upon acceptance of a complete application for DRC consideration, a project shall be set on the first available agenda for DRC review. The applicant and any person requesting notice will be notified at least ten (10) days prior to the meeting. In addition, the property shall be posted at least ten (10) days prior to the meeting. The applicant may appeal the DRC's decision to the DRB if a written appeal is submitted to the Community Development Director within ten (10) days of the action by the DRC.

5.7.5 Findings

The DRB or DRC, where authorized, shall make the following findings before approving a Development/Design Review application:

- That the proposed project is consistent with the General Plan; and that the proposed use is in accord with the objectives of the La Puente Downtown Business District Specific Plan and the purposes of the district in which the site is located.
- That the proposed use is in compliance with each of the applicable provisions of the La Puente Downtown Business District Specific Plan; and, that the proposed use, together with the conditions applicable thereto, will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.

5.7.6 New Applications Following Denial

Following the denial of a Development Review application, no application for the same or substantially the same use or the same or substantially the same site shall be filed within one (1) year from the date of denial.

6.0 PLAN IMPLEMENTATION

The following implementation strategies are actions which are beyond the power of this Specific Plan and their enactment will occur by "separate acts" following the adoption of this Specific Plan.

6.1 SPECIAL EVENTS

Beginning in the first and second phases of development, it is highly desirable that an Events Coordinator position be established on either a paid or volunteer basis or a volunteer committee created to work closely with the City of La Puente Chamber of Commerce and the City's economic development group to organize monthly and annual public and civic events. These events should be held in the DBD, with the majority of the activities and events focusing on the public park/plaza and Main Street. These events could include automotive shows and exhibits, weekly farmers markets, cooking contests, fairs, carnivals, festivals, special runs and marathons, and other similar activities that will generate interest and attract shoppers into the DBD.

6.2 SPECIAL INCENTIVES

Special incentives, to be established by the City at a later date and by separate action, for the development, redevelopment, restoration and intensification of the desired uses within the La Puente DBD may include:

- City-supported loan and grant programs.
- City-supported commercial rent incentives.
- Waiver of City development fees.
- A density bonus for any project consisting of a minimum 15,000 square foot parcel.

The City should encourage lot consolidations and shared, joint accesses and parking. Property owners participating in such lot consolidations and proposing uses recommended within their sub area, will increase their opportunity to partake in most or all of the above special incentives. Incentives to attract high sales market businesses also should be investigated.

Incentives could include administrative or technical support for the project as well as possible financial inducements. In such cases, one or more of the following devices may be appropriate:

- Permitting flexible development standards.
- Waiver of selected development fees.

❖ City of La Puente Downtown Business District Specific Plan ❖

- Assisting with background market research.
- Sales tax refunding.

The Specific Plan recognizes that problems of marginal land use are often associated with a pattern of small commercial land parcels. Highly fragmented ownership and land use arrangements also seriously hinder plans for renovation or expansion of businesses operating within the DBD planning area.

The Specific Plan will establish an incentive program for small lot consolidation within the DBD. Assistance to support lot consolidation will apply only under certain conditions enumerated in the development regulations section of this Specific Plan.

The State of California has required that all masonry buildings meet State earthquake building and safety codes within three (3) years. Because many of the buildings within the DBD are constructed of masonry, these buildings will need to either be renovated to meet the earthquake codes or demolished. The City should encourage property owners to upgrade and renovate existing buildings to meet the required codes or, if rehabilitation is not economically feasible, to demolish the structures and construct new structures. The new structures will be required to undergo Design Review; this will enable the City to ensure that all new buildings contribute and enhance the small-town character and aesthetics of the downtown.

Relaxation of development standards and/or provision of financial inducements would be the mechanisms used to stimulate lot consolidation and eventual refurbishment of existing uses and/or establishment of new development. These mechanisms would be applied on a case-by-case basis, depending on the current pattern of development and the new proposals submitted. Incentives should be expanded proportionately in respect to the number of owner participants and the number of corresponding lots ultimately consolidated.

6.3 SMALL LOT CONSOLIDATION

It is the expectation of this Specific Plan that development within the DBD will occur, for the most part, on a lot-by-lot basis. Where feasible, this project encourages lot consolidation in order to allow for more flexibility in possible land uses, building designs and parking lot layouts, while minimizing curb cuts and ingress and egress points into parking areas in order to encourage the smooth flow of traffic on streets within downtown La Puente. Therefore, because it is likely that some, if not all, of the development within the DBD will occur on a lot-by-lot basis, rather than on a sub area scale, the ultimate development of the DBD (under the policies, development regulations and design guidelines of this Specific Plan) is likely to vary in building design and site plan from the *Illustrated Site Plan* (see Figure 7), which is a conceptual depiction of one preferred scenario that could be implemented under the auspices of this Specific Plan.

6.4 ECONOMIC DEVELOPMENT STRATEGIES

This section outlines various economic development strategies which may be implemented subsequent to Specific Plan adoption. These efforts are intended to provide the framework for financing planning area, public and private improvements which can bring the circulation and urban design components of the plan into reality and establish other mechanisms which will ultimately promote the viability of the DBD.

6.4.1 Creation of the DBD Redevelopment Project Area

The City of La Puente has supported the concept of redevelopment through formation of a project area for the DBD. The central aim of this policy is to set in place a financing vehicle for underwriting street and intersection reconstruction and associated public design treatments. It also will serve as a tool for possible Joint public/private participation in project area rehabilitation and promotion of new development.

Project area formation should be among the first actions of the Specific Plan to be implemented. It should be initiated within the first calendar year following plan adoption. The legal requirements for preparing and adopting a redevelopment plan for the DBD are outlined in Articles Four and Five (Sections 33300-33375) of California's Community Development Plan.

Redevelopment should be used in the DBD primarily for financing street improvements and associated design features. It is understood that the use of eminent domain powers are not intended at this time to be included in any adopted Redevelopment Plan.

6.4.2 Attraction of Desired Uses

An economic market analysis prepared during this Specific Plan process identified a number of retail activities which are not adequately represented in the DBD. Efforts to attract these land uses should be made to the extent that they would compliment, rather than complete directly with existing land uses found within the DBD.

Attracting high sales markets will be an on-going process. However, once appropriate infrastructure facilities are established and parcels are potentially consolidated to accommodate these uses, the greater the potential exists for capturing high sales volume markets. Types of business which are now underdeveloped in the DBD and targets for future marketing efforts include, but are not limited to, a small scale grocery store, hardware store and drug store. Special incentives to attract these uses to the DBD are discussed in Section 6.2, above.

6.4.3 Small Business Assistance Program

New business promotion and redevelopment activity does not mean that the needs of existing DBD retail and service outlets will be neglected. The Downtown

Plan is committed to strengthen the operation of existing uses which are viable contributors to serving neighborhood and community wide consumer demands.

In order to assist the small business sector, the City of La Puente will work toward the establishment of a local development corporation in conjunction with the La Puente Chamber of Commerce. The function of this non-profit organization would be to stimulate the health and expansion of small businesses through low-interest loans or grants. This funding could support building refurbishment and rehabilitation, leaseholder improvements, renovation, and modernization, purchase of machinery and equipment, land acquisition, and/or relocation assistance.

Small Business Administration (SBA) regulations set criteria for businesses which may receive financial aid. The concept of "shared risk" is advanced, as the SBA program will provide loans only up to 40 percent of project costs; owner equity and conventional bank/private sector lending must make up the remainder.

The non-profit development corporation will be an autonomous unit, not related to City government. However, its management board should include one or two representatives of City departments concerned with planning and community development. Board composition typically includes seven to eleven persons reflecting various business and community perspectives. Through this program, existing services for the DBD can be strengthened, entrepreneurship supported, job growth can occur and the DBD tax base can be expanded.

6.4.4 Implementing and Financing Alternatives

Methods which could be used to provide for development of public facilities and their maintenance within the DBD include:

A. *Assessment District:*

California law authorizes a variety of assessment procedures which can be employed to pay for development of major capital expenditures through the assessment district. Financing is arranged from bonds, and obligations for payment of the financing is shared by those within the assessment district. Approval of the assessment district requires a substantial majority of property owners to agree to the formation of the

district. This system works best for projects of limited scope and purpose. The advantage of this financing mechanism is that it provides for a means to complete an overall expansion of a project at one time, thus assuring logical development of and obtaining the efficiency of completion of the project in one setting.

B. Maintenance District:

Because the landscape system for the DBD will require maintenance, some form of maintenance will be necessary. A maintenance district is a common practice for assuring for the on-going revenues to support their service. Maintenance districts can include a large area of multiple parcel ownerships with an annual assessment being made to cover the necessary maintenance of systems within the planning area.

C. Parking Districts:

Owners of two or more contiguous properties may form a joint-development parking district and develop a comprehensive parking plan that will allow them to coordinate their parking requirements, not necessarily tied to individual zoning requirements. Another type of parking district, a public off-street parking district, may be created in which parking spaces shall be credited to the uses on parcels in the assessment district in proportion to the assessment levied against each parcel or value of land contributed by the owners of such parcel. These types of parking districts are discussed more fully in Section 5.3.1, *Parking Criteria*, of this Specific Plan.

D. Redevelopment Law:

This act allows communities to utilize tax increment financing to carry out redevelopment activities by applying tax increments obtained in the project area to finance planning, administrative, acquisition and improvement activities. The act permits a redevelopment agency to finance land acquisition for public purpose, construction of public facilities, and administrative legal, planning and engineering costs related to the project. The City redevelopment agency would designate a project area and would prepare a redevelopment plan. The agency, then would apply the tax increments derived in the project area to pay the debt service on the bonds. Tax increments are those tax revenues received during the base year preceding adoption of a redevelopment plan.

6.5 PROJECT PHASING

Implementation of the La Puente Downtown Business District Specific Plan is anticipated to occur over a 10 to 20 year time period in approximately four phases (see Figure 13, *Project Phasing Plan*). These phases may occur independently of each other or they may occur concurrently as demand requires and resources permit. Table 3, *Phasing Program*, on the following page, includes a breakdown of each of the four projected phases by land use. This Phasing Program is provided as a reference guide for decision-makers to use in reviewing and approving projects. It should be understood that the actual phasing and implementation of development within each sub area may vary somewhat from this Phasing Program depending upon changing economic, market and other unforeseen conditions.

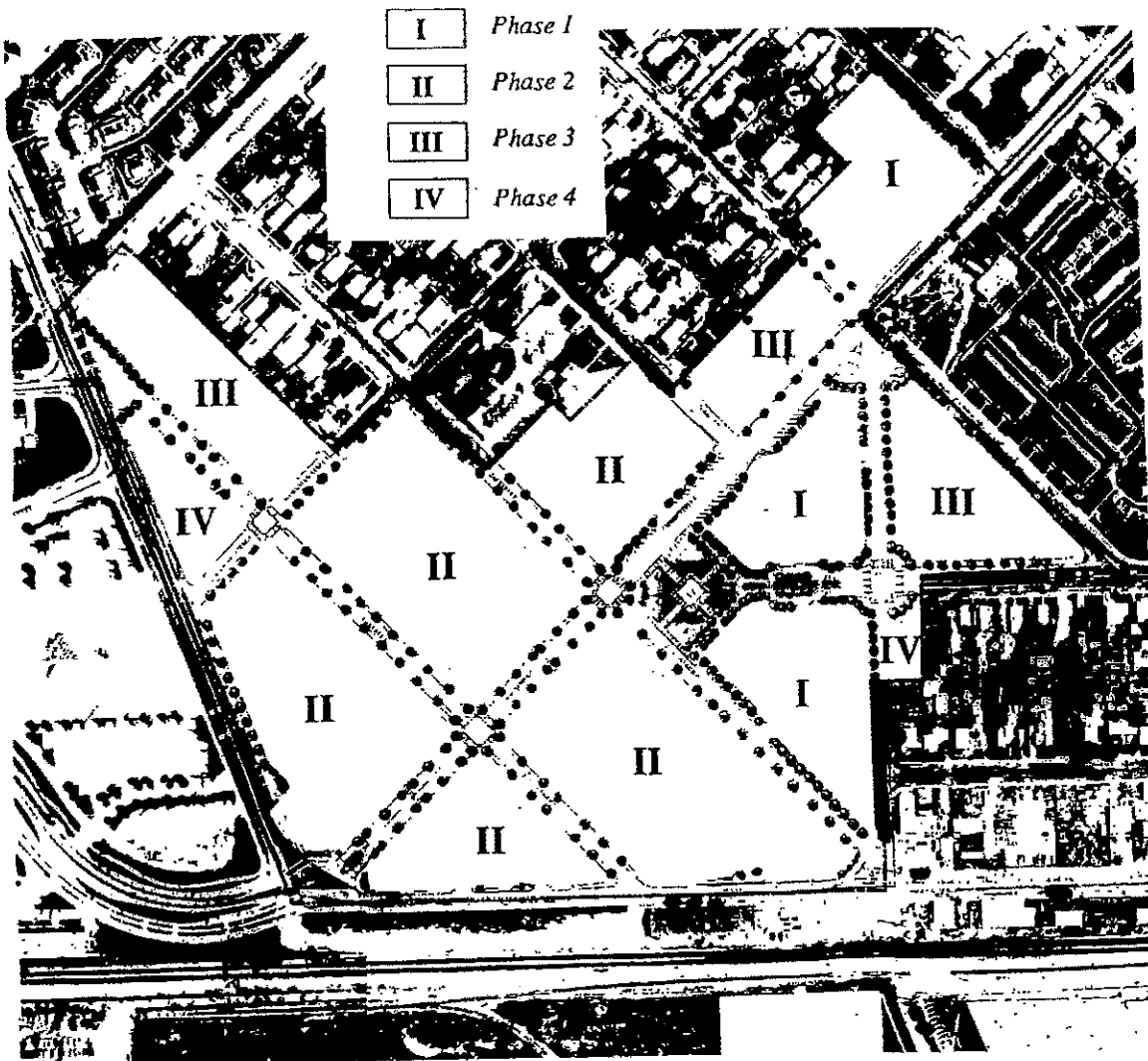
Although most of the planned multi-family residential units are shown as developing in Phase III of the Phasing Program, residential uses should be allowed to develop earlier if at all possible. Proposals for residential development should be given priority since development of dwellings within the DBD will ultimately help spur a need for specialty commercial and community service uses.

**TABLE 3
PHASING PROGRAM**

PHASE	LAND USE	MAXIMUM SQUARE FOOTAGE	TOTAL DWELLING UNITS
I	Senior Citizen Center	10,150	-
	Community Services	7,200	-
	Civic Center ^(a)	37,680	-
II	Commercial	115,800	-
	Office/Residential	62,400	-
	Office/Light Industrial	40,750	-
	Community Services	33,000	-
	Multi-Family Residential	-	25
III	Woman's Club	9,995	-
	Multi-Family Residential	-	203
IV	Office	23,400	-
	TOTAL	340,375	228

^(a) Civic Center complex includes City Hall, County Library, and County Health Center

Legend



PROJECT PHASING PLAN
FIGURE 13



7.0 GENERAL PLAN CONSISTENCY

The City of La Puente's General Plan Identifies goals, objectives, policies and action programs in all of the areas mandated by State law. The following are pertinent goals, objectives, policies and actions of the General Plan and a discussion of how the Specific Plan conforms to each component.

GOAL 1: To enhance the visual and economic image of the Downtown area.

- Objectives:
- a) Enhance the commercial appeal of the historic Downtown shopping area.
 - b) Ensure that the commercial area within the Downtown is pleasant in atmosphere, efficient in operation and safe for pedestrians and motorists.
 - c) Promote a balanced and functional mix of land uses consistent with community values.
- Policies:
- a) To continue revitalization efforts in the Downtown area.
 - b) Encourage greater merchant participation in Downtown planning programs.
 - c) Improve vehicular movement Downtown.
- Actions:
- a) To pursue through discussions with the City of Industry, the reopening of Stimson Avenue for improved circulation in the Downtown.
 - b) Create a Downtown Business District land use category and implementing zone.
 - c) Encourage the private development of a small, full service neighborhood market within the Downtown Business District.
 - d) Consider a grant application for the federal Main Street program which provides funds to refurbish buildings in older commercial areas.

Consistency: The La Puente DBD Specific Plan is designed to enhance the visual and economic image of the downtown area by providing a viable mix of commercial, office, community service and residential uses which meet the needs of a diverse population. The Specific Plan provides guidance for new development that is compatible with existing uses. The downtown American character will be restored through the rehabilitation, renovation and intensification of existing commercial, office and residential uses.

Vehicular movement in the downtown will be unproved by modifications to Central Avenue, Main Street and First Street "Pinched" intersections will provide pedestrian safety by slowing traffic and providing better views on street corners. Other streetscape improvements include enhanced paving treatments, diagonal parking stalls and planting pockets provided at regular intervals.

The Specific Plan development regulations have established land use categories and implementing zones within the DBD. Redevelopment and intensification in the downtown will require cooperation between the public and private sectors.

GOAL 2: To maintain adequate Infrastructure to support La Puente's various land use categories.

Objectives: a) To ensure that there is adequate infrastructure capacity through continued upgrading and replacement of facilities.

b) Maintain adequate open space.

Policies: a) Continue to Improve infrastructure through the use of CBDG and general fund revenues.

b) Maintain the recently completed Infrastructure projects within the Downtown.

Actions: a) Develop a Capital improvement program.

b) Continue monitoring of existing infrastructure.

Consistency: Section 3.4 of the La Puente Downtown Business District Specific Plan addresses public facilities and infrastructure necessary to support the project area. Since the DBD is an existing urbanized area, infrastructure facilities are already in place. The sewer, water and drainage systems are sized to accommodate any increases in intensity of land use within the downtown. The City of La Puente shall be responsible for maintaining the recently completed infrastructure improvements within the DBD.

GOAL 3: Encourage commercial growth to meet existing and future needs.

Objectives: a) Identify and designate commercial land in convenient locations to meet the existing and future needs of the community.

❖ *City of La Puente Downtown Business District Specific Plan* ❖

- b) Identify new residential opportunities in the downtown area that will have a positive impact on stimulating the expansion of commercial growth in this area.
- Policies: a) Rezoning shall be considered where it would encourage commercial growth.
- b) Promote reinvestment of commercial property.
- Actions: a) Continue to implement the Economic Development Fund Gap Program which facilitates revitalization and new construction in declining commercial areas.
- b) Continue counseling to improve business management and opportunities within the DBD and other commercial areas, and other programs funded by the City's Economic Development Program.
- c) Amend the City's zoning ordinance (through the Specific Plan process) to reflect the land use categories of the Land Use Plan.
- d) Continue code enforcement activities to maintain commercial areas in an acceptable condition.
- e) Continue economic and redevelopment incentive programs through various state and federal grant assistance programs.
- Consistency: The La Puente DBD Specific Plan promotes high quality, attractive and functional site planning and architecture for commercial, office, residential and community service development within the DBD. Commercial uses are planned to develop in convenient locations to meet the existing and future needs of the community. Commercial, retail, office and light industrial uses in the downtown area will be balanced with high density residential land uses. These residential uses will have a positive impact on the expansion of commercial growth in the DBD. Residential land uses could include townhomes, condominiums, senior housing and SROs.
- GOAL 4: To guide the revitalization of residential and commercial areas in a manner that is aesthetically sensitive to the natural environment and compatible with existing development.
- Objectives: a) Promote commercial developments that are attractive to the consumer.
- b) Encourage Historic Preservation in the downtown core and in older residential neighborhoods.

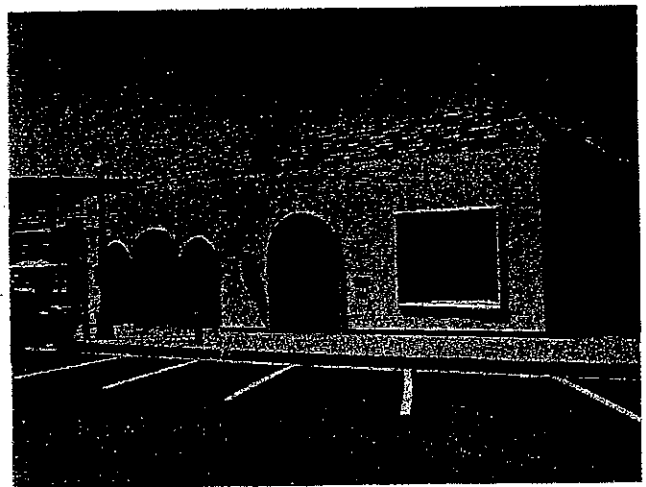
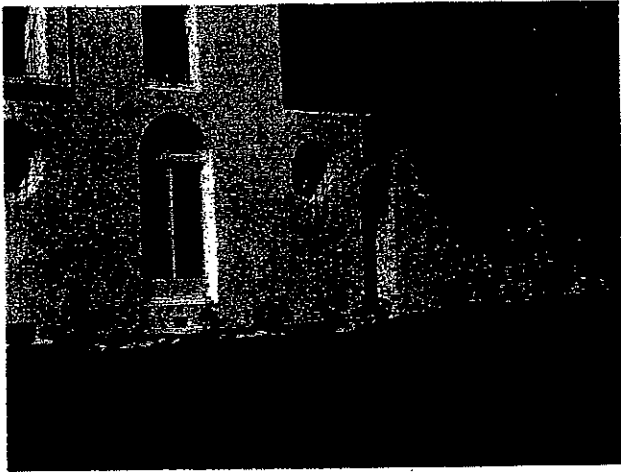
- c) Provide for commercial environments which will contribute positively to the City's appearance.
- Policies:
 - a) All commercial and residential developments shall be subject to a plan review that will ensure harmonious design.
 - b) The City's existing Design Element of the General Plan shall be expanded to include any or all development.
- Actions:
 - a) Establish architectural design standards as part of the City's Zoning Ordinance (to be implemented through the Specific Plan process).
 - b) Amend the City's Development Review Board authority to provide better design guidance to the Board.
- Consistency: This Specific Plan includes specific Development Regulations (Section 4.0), as well as a set of Design Guidelines (Section 5.0). Whenever feasible, the La Puente Downtown Business District Specific Plan encourages the preservation of existing and historic buildings within the DBD. However, because of State requirements regarding the earthquake safety of masonry buildings, it may not always be cost effective or possible to preserve or renovate existing structures. The Design Guidelines and Development Standards in this document are intended to provide guidance to builders, developers and City decision-makers in designing, approving and building structures that preserve and enhance the small-town character of downtown La Puente.

APPENDIX

PHOTOGRAPHS ILLUSTRATING ARCHITECTURAL DESIGN
ELEMENTS THAT ARE ENCOURAGED WITHIN THE
DOWNTOWN BUSINESS DISTRICT SPECIFIC PLAN AREA
FOR RESIDENTIAL DEVELOPMENTS

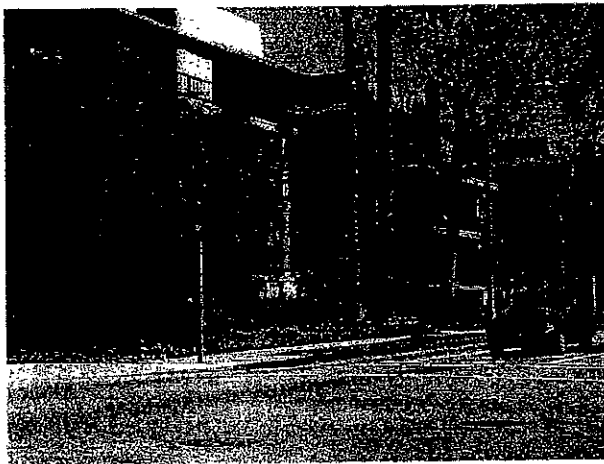
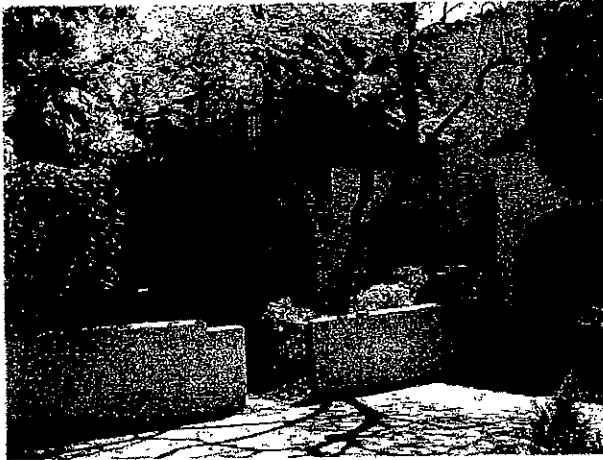
SPANISH ARCHITECTURE

Arches



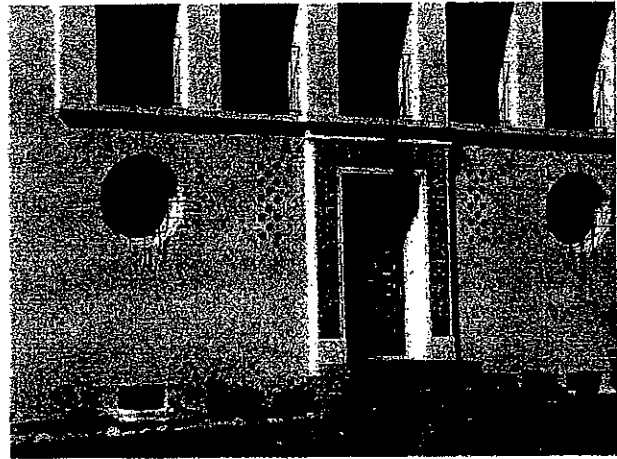
SPANISH ARCHITECTURE

Balconies



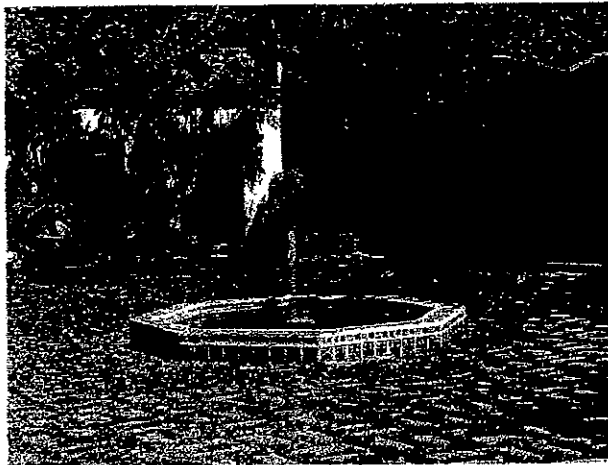
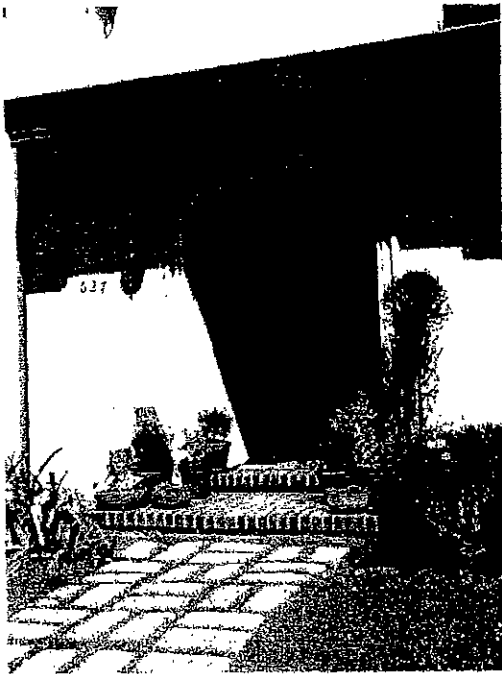
SPANISH ARCHITECTURE

Doors



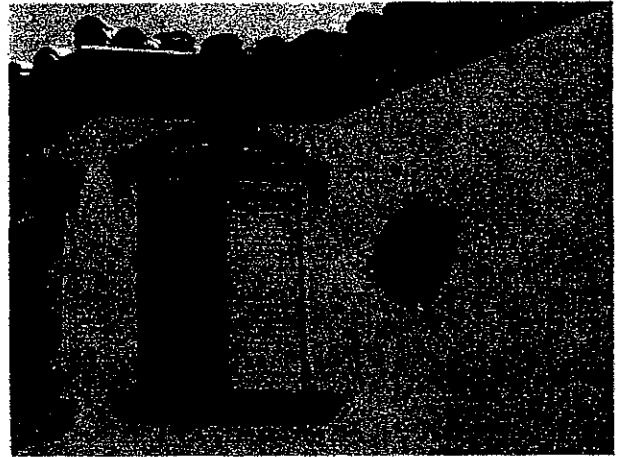
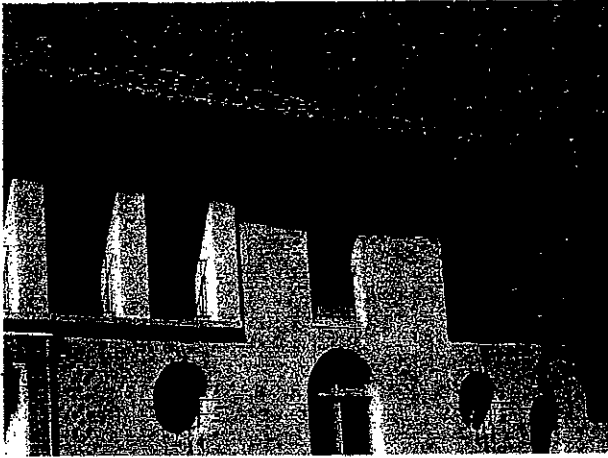
SPANISH ARCHITECTURE

Patios & Paving



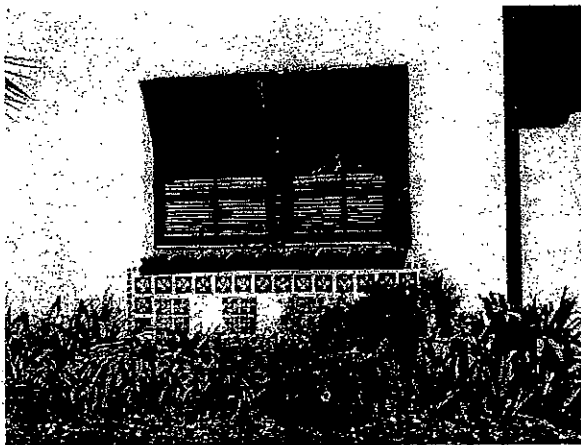
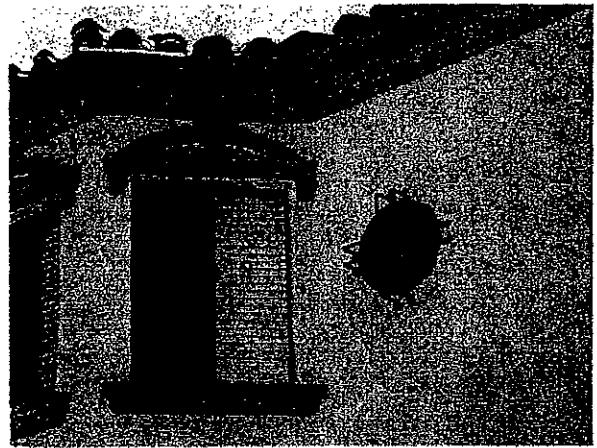
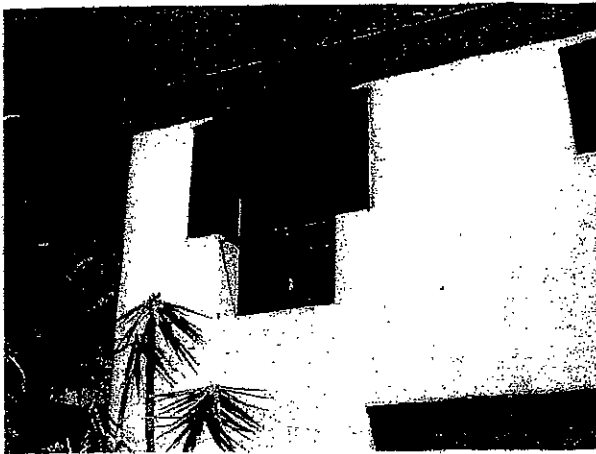
SPANISH ARCHITECTURE

Roofing



SPANISH ARCHITECTURE

Windows



DOWNTOWN SPECIFIC PLAN

LAND USE MAP

